

City of Rosemead

# Garvey Avenue Specific Plan

Adopted February 13, 2018

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# Chapter 1

## INTRODUCTION

### IN THIS CHAPTER

#### 1.1 Garvey Avenue Specific Plan Vision and Objectives

#### 1.2 Purpose

#### 1.3 Contents and Organization of Garvey Avenue Specific Plan

#### 1.4 Public Engagement Process

#### 1.5 Relationship to City Plans and Programs

#### 1.6 Legal Authority

### 1.1. GARVEY AVENUE SPECIFIC PLAN VISION AND OBJECTIVES

Slow economic growth contributed to structures along the Garvey Avenue corridor falling into disrepair, along with building and lot vacancies. Garvey Avenue has accumulated a mix of scattered commercial and light industrial land uses, which can occur as the result of a loss of significant anchor land use or lack of targeted economic development and/or planning efforts. Current uses include underperforming strip malls, small/privately owned restaurants, automotive sales and repair shops, and small industrial uses and warehouse facilities.

Garvey Avenue, a well-traveled arterial road and an alternative to the I-10 freeway, has the potential to become a noted shopping and entertainment destination of local and subregional importance.

The Garvey Avenue Specific Plan establishes a vision and objectives for new development, a diversity of land uses, and public improvements along the 1.2-mile section of Garvey Avenue that extends from Charlotte Avenue to New Avenue.

## Vision for the Garvey Avenue Specific Plan:

*To revitalize the Garvey Avenue corridor from a commercial/industrial area into a vibrant commercial, residential, and mixed-use district, with a compliment of local and subregional serving retail, entertainment, and service businesses, office space, and community uses, all tied together with public improvements that create a vibrant and enjoyable pedestrian environment.*

The Specific Plan defines actions to be taken to reinvigorate the corridor into one that better serves the surrounding area and creates an attractive and convenient shopping and entertainment destination for local and subregional residents. It defines actions that create an attractive gateway into Rosemead, consistent with the General Plan’s goals. Key Specific Plan objectives are:

- Create a neighborhood “main street” that will serve as a focal point for the neighborhoods surrounding Garvey Avenue.
- Provide new opportunities for commercial and residential uses in mixed-use settings.
- Facilitate opportunities for catalytic developments that provide desired retail, entertainment, and service businesses, employment opportunities, and support the local community.
- Provide for the gradual phasing out of industrial uses that create conflicts with the surrounding neighborhoods.

- Support design that contributes to the enhanced character of the City and Garvey Avenue, in particular.
- Create “place making” public plazas, gathering spaces, parks, and parklets that serve as focal points for the corridor.
- Enrich the pedestrian and bicycle environment along Garvey Avenue through well-designed and appropriately scaled paths and pleasing streetscapes.
- Encourage investment, maintenance, and pride in the Garvey Avenue Specific Plan area.

The City recognizes that significant changes will not happen immediately or within the next year. The goal is to create a framework for change and to inspire private reinvestment along the corridor that includes the rehabilitation of aging buildings and establishment of new buildings and uses that can achieve the vision and objectives.



Some existing uses along Garvey Avenue, signage, and landscaping serve as placemaking features or local landmarks.

## 1.2. PURPOSE

The Garvey Avenue Specific Plan provides a long-term strategy for revitalizing and increasing the development potential of an underutilized commercial and light industrial corridor in Rosemead, California. The corridor has the potential to become an iconic and attractive area for shopping and entertainment, with improved access to destinations and an inviting environment for bicycle and pedestrian activity. The Garvey Avenue Specific Plan will help mend disjointed development and contribute towards a more unified aesthetic and a greater sense of place within the planning area.

The Garvey Avenue Specific Plan will establish land use, transportation, infrastructure, and urban design strategies that seek to attract investment into the corridor, and promote well-balanced retail development, mixed-use development, and active public spaces. The Garvey Avenue Specific Plan also identifies public improvements that will further enhance the corridor's appearance and character.

## 1.3. CONTENTS AND ORGANIZATION OF GARVEY AVENUE SPECIFIC PLAN

The Garvey Avenue Specific Plan is organized as shown in Table 1.1.

## 1.4. PUBLIC ENGAGEMENT PROCESS

The Garvey Avenue Specific Plan builds upon input provided by community stakeholders within the Garvey Avenue Specific Plan Area. The City organized an Ad Hoc Committee that met 13 times throughout the planning process. Stakeholder interviews and community surveys were conducted with a diverse representation of community business owners and residents. Meetings with relevant City departments and the City Council also contributed feedback that was incorporated into the Garvey Avenue Specific Plan. The information gathered provides valuable insight about the strengths, weaknesses, opportunities, and concerns within the corridor from the perspective of diverse community stakeholders who operate within it.

## Ad Hoc Committee

The five-person Ad Hoc Committee was comprised of the following representatives:

- City Council (2 members)
- Planning Commission (1 member)
- Traffic Commission (1 member)
- Beautification Committee (1 member)

During the 13 meetings convened, the Committee discussed a Corridor vision, evaluated land use alternatives, provided input to the proposed land use plan, and reviewed the Draft Garvey Avenue Specific Plan.

## Stakeholder Interviews

A total of eight of interviews were conducted with local business community members, local residents, and Garvey School Board members on July 31, 2014. Interviewees were asked seven questions in which they identified what they like about the project area, what should change and how should it be changed, and the opportunities and issues facing the corridor.

**Table 1.1: Garvey Avenue Specific Plan Contents**

Chapter	Contents
1: Introduction	The Introduction chapter explains the role and function of specific plans, and the vision and purpose of the Garvey Avenue Specific Plan. This chapter describes the Garvey Avenue Specific Plan’s relationship to the City’s General Plan, Zoning Ordinance, and other guiding documents. This chapter, also, summarizes the public engagement process’ findings.
2: Context and Conditions	This chapter provides the planning context — location, setting, and context of planning issues.
3: Land Use, Zoning, and Development Standards	This chapter sets forth site planning, building, parking, architectural, and open space provisions.
4: Opportunity Projects and Sites	The Opportunity Project and Sites chapter provides land use and development scenario examples for three opportunity sites. These sites have the potential to serve as catalysts for new development and economic revitalization. In addition, this Plan foresees significant streetscape changes that will transform the pedestrian and bicyclist experience in the Garvey Avenue Specific Plan area.
5: Infrastructure (Mobility, Utilities, Public Services)	This chapter includes the conceptual vehicular and non-vehicular circulation plan, utilities, and public services servicing the Garvey Avenue corridor.
6: Implementation	This chapter discusses the actions and programs anticipated for the successful implementation of this Specific Plan. It also addresses the Plan’s administration and amendment process.

## Community Survey

The planning team gathered further input through the distribution of a community survey at a public City event (National Night Out – August 5, 2014) and via the City’s website. This survey gave Rosemead residents and business owners the opportunity to voice their opinion on the issues, opportunities, and concerns relevant to their perspective of the Corridor today. A total of 152 surveys were collected.

## City Department Meetings

Nine staff members representing seven key City departments convened for a July 16, 2014 meeting. The departments represented include: City Administration, Public Works Department, Finance Department, Community Development Department, and County Sheriff’s Department. This meeting discussed the Corridor’s opportunities and constraints.

## City Council Workshop

On January 13, 2015 the City Council was provided with an update presentation on the progress of the Ad Hoc Committee towards the development of the Garvey Avenue Specific Plan and EIR. The presentation included a summary of the key development principals and catalytic sites that would shape the plan.

## City Council/Planning Commission/ Traffic Commission/Beautification Committee Joint Special Meeting

A special meeting/study session was conducted with the City Council, Planning Commission, Traffic Commission, and Beautification Committee on December 12, 2017 to review and respond to questions regarding the Draft Specific Plan, changes as proposed in the Errata, and the EIR.

## Findings

The information gathered through the community participation paints a rich picture of both the current state and desired future for the Garvey Avenue Corridor.

## Strengths

- Well served by public transportation.
- Significant number and size of vacant or underutilized properties provide an opportunity to create a one-stop shopping area that provides a variety of experiences.
- Substantial number of pedestrians and bicyclists.

## Weaknesses

- Lack of public gathering spaces, open space, and park/parklets.
- Limited roadway right-of-way yet additional infrastructure capacity may be needed.
- Walkability needs to improve.
- Limited range of goods and services available in the neighborhood.



Garvey Avenue is a transit and bicycle corridor.

- Landscaping and lighting needs to be more abundant.
- Need for nightlife.

## Opportunities

- Create an exciting, vibrant pedestrian oriented area that benefits local and subregional interests.
- Balance long term land uses serving nearby residents and development community needs, while providing local economic stability.
- Attract high end retail, restaurants, and businesses to generate local employment opportunities.
- Create a special place that creates a commonality between urban design, land use, and streetscape.
- Coordinate bike lanes and pedestrian paths throughout the Garvey Avenue corridor.

## 1.5. RELATIONSHIP TO CITY PLANS AND PROGRAMS

### **Existing 2010 General Plan Update**

Any land use approach for Garvey Avenue must be consistent the primary goals of Rosemead’s General Plan, which was last comprehensively updated in 2010. The 2010 General Plan Update provides a citywide approach to planning for future development, and outlines goals and strategies for major commercial corridors such as Garvey Avenue. It considers the seven required General Plan elements, including land use, circulation, housing, open space, conservation, noise, and safety, in six chapters, with conservation and open space combined into a Community Resources element. The General Plan identifies a set of goals, policies, and actions related to each of the chapters.

Five of the six primary vision elements of the 2010 General Plan Update apply to Garvey Avenue, including:

- Enhance the commercial areas along key corridors, and most specifically Garvey Avenue and Valley Boulevard.

- Enhance parks and recreational space in underserved neighborhoods.
- Accommodate the demand for high quality mixed-use development that can contribute to commercial growth and enhance opportunities for higher-density residential development.
- Protect homeowner investments and the availability of well-maintained, relatively affordable housing units.
- Minimize the impact of traffic associated with growth within the San Gabriel Valley and broader region.

The goals established in the 2010 General Plan Update relating to potential land use decisions along the Garvey Avenue corridor include the following:

### Land Use Element

- Goal 2: Expand opportunities for concentrated commercial and industrial uses that contribute jobs and tax revenues to the City.

- Goal 3: Create vibrant, attractive mixed-use development.
- Goal 4: Assure a financially healthy City that can meet residents’ desire for public services and facilities.
- Goal 5: Use targeted land use changes that improve housing and economic opportunities for residents and businesses and achieve City fiscal and environmental objectives.

## Circulation Element

- Goal 1: Maintain efficient vehicular and pedestrian movements throughout the City.
- Goal 2: Develop infrastructure and service to support alternatives modes of travel.
- Goal 3: Assure vehicular traffic associated with commercial and industrial uses does not intrude upon adjacent residential neighborhoods.
- Goal 4: Provide quality commercial and industrial development with adequate parking for employees and visitors.

## Resource Management Element

- Goal 1: Provide high-quality parks, recreation, and open space facilities to meet the needs of all Rosemead residents.
- Goal 2: Increase green space throughout Rosemead to improve community aesthetics, encourage pedestrian activity, and provide passive cooling benefits.
- Goal 4: Make effective contributions to regional efforts to improve air quality and conserve energy.

## Noise Element

- Goal 1: Ensure effective incorporation of noise considerations into land use planning decisions.
- Goal 2: Reduce noise impacts from transportation sources.
- Goal 3: Effectively implement measures to control non-transportation noise impacts.

## Housing Element

- Goal 2: Encourage the development of a range of housing types in a range of prices affordable to all Rosemead residents.
- Goal 3: Encourage the maintenance and upgrading of existing housing stock to ensure a decent, safe, and sanitary home for all Rosemead residents.

## **Garvey Avenue Master Plan**

The Garvey Avenue Master Plan is a guiding document for development along Garvey Avenue; it should be a key reference point for the Garvey Avenue Specific Plan. The Master Plan is intended to address “aesthetic and parking issues that property and business owners face in planning commercial and retail improvements.” Its vision for Garvey Avenue: “In the year 2020, Garvey Avenue will be recognized as a vibrant corridor with clean storefronts and visible pedestrian activity. Garvey Avenue will create a sense of ‘Small Town’ community pride where a variety of opportunities can be discovered for families and friends.”

Its key objectives include the following:

- Upgrade the image and appeal of the Garvey Avenue corridor by coordinated public and private improvements.
- Entice and create convenience for patrons to stop and shop along the Garvey Avenue commercial corridor.
- Create energy along Garvey Avenue by creating pedestrian activity and sidewalk cafes with outdoor seating.
- Develop great place-making areas that will define the Garvey Avenue commercial corridor.
- Create adequate parking facilities and improve traffic flow along the commercial corridor.

Promote and encourage the highest and best use of under-utilized properties.

- Utilize landscaping as an integral component to overall project design.
- Consider scale and character of adjacent uses and demonstrate

sensitivity to the influences of the surrounding area.

- Encourage private rehabilitation through application of the Garvey Avenue Master Plan for new and existing businesses.
- Strengthen the Property Maintenance Ordinance to rigorously enforce property maintenance standards for commercial and industrial properties.

Its action plan includes:

- Revise the Property Maintenance and Sign Ordinances to clearly address commercial property maintenance (e.g. clean storefronts, parking lot improvements, sign rehabilitation, etc.).
- Recognize that different parts of the corridor have special characteristics; develop programs to strengthen and reinforce them.
- Direct project designs that will promote pedestrian-friendly projects with public spaces and lively street

fronts where people can meet and interact.

- Encourage developments as a means of upgrading established uses and developing vacant parcels along Garvey Avenue.
- Attract private investment to revitalize older commercial uses that will reinforce and create synergy along the Garvey Avenue commercial corridor.
- Enforce high quality commercial building and site design while allowing increased intensities of use along the corridor where appropriate.
- Require economic feasibility studies on large development sites to ensure projects are economically sustainable.
- Establish a well-balanced and carefully planned collection of signature retail anchors, general retail outlets, casual to upscale restaurants, and upscale overnight accommodations.

- Discourage the development of commercial properties that contain a random mix of incompatible uses.
- Encourage the placement of parking areas to be located behind structures and out of sight from the public right of way.
- Promote lively and attractive ground-floor retail uses that will create revenues needed to provide for City services and City's tax base.

### Existing Zoning Code

The City's existing zoning code describes development standards and allowable uses by zoning district. Development standards identified include setbacks, lot area, lot width, density, floor area ratio, site coverage, landscaping and open area requirements, height limits, storage, and parking.

### City of Rosemead Mixed-Use Guidelines

Rosemead's Mixed-Use Guidelines, released in 2012, provide a context and strategy for mixed-use development throughout Rosemead. To the extent that development within the Garvey Avenue Specific Plan area is proposed to be

mixed-use, the Mixed-Use Guidelines will offer important guiding principles to follow.

The Mixed-Use Guidelines include sections devoted to:

- Public realm and pedestrian environment
- Site design
- Building design
- Building height
- Storefronts
- Lighting
- Common areas/open space
- Compatibility with adjacent properties
- Parking
- Access

### 1.6. LEGAL AUTHORITY

The Garvey Avenue Specific Plan is enacted pursuant to Sections 65450 through 65457 of the California Government Code, which authorizes local governments with certified General Plans to prepare and adopt specific plans. The Specific Plan is adopted by the City Council by ordinance and, thereby, establishes the zoning regulations for land use and development within the Specific Plan area.

Specifically, these sections of the Government Code establish that a specific plan must, at a minimum, address the following:

- a. A specific plan shall include text and a diagram or diagrams that specify all the following in detail:
  1. The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
  2. The proposed distribution, location and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.
  3. Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resource, where applicable.

4. A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out paragraphs (1), (2), and (3).
- b. The specific plan shall include a statement of the relationship of the specific plan to the general plan.

**Strong urban site design features – trees, planters, street furniture, outdoor cafes, and awnings – will make the Garvey Avenue Corridor walking experience much more pleasant.**





# Chapter 2

## CONTEXT AND CONDITIONS

### IN THIS CHAPTER

- 2.1 Community History**
- 2.2 Regional Context**
- 2.3 Development Patterns**
- 2.4 Land Use**
- 2.5 Urban Design**
- 2.6 Mobility**
- 2.7 Infrastructure**
- 2.8 Public Services and Facilities**

### 2.1. COMMUNITY HISTORY

Rosemead was first settled by Gabrieleno Native Americans and then, later, by the Spanish, who established a mission in the San Gabriel Valley in 1771.<sup>1</sup> Much of the southern part of Rosemead was part of Rancho Potrero Grande, one of the major original Spanish and Mexican Ranchos of Los Angeles County (Figure 2.1). Anglo-American immigration began after the Treaty of Guadalupe ended the Mexican-American War in 1848. Among the early white settlers was Leonard Rose and his wife; they established a 600-acre ranch between what is now Rosemead Boulevard and Walnut Grove Avenue. Rose called his ranch Rose's Meadow, which was later changed to Rosemede, and, finally, Rosemead, giving the City its name.

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<sup>1</sup> City of Rosemead. 2010 General Plan Update

**Figure 2.1 Map of the Old Spanish and Mexican Ranchos located in Los Angeles County**



Garvey Avenue is named for Richard Garvey, Sr., a 19<sup>th</sup> Century mail rider who delivered mail between Arizona and California. Garvey purchased land along his route between what is now Atlantic Boulevard and San Gabriel Boulevard. He eventually gave a portion of his land to create Garvey Avenue.<sup>2</sup>

Rosemead was predominantly an agricultural community until 1959, when the city was incorporated. From 1960 to 1970, the population exploded, from 15,000 to nearly 41,000. This growth was a result of suburban tract house development accommodating Los Angeles area workers and their families. The 2014 population was approximately 54,700;<sup>3</sup> official predictions anticipate Rosemead will grow to just under 60,000 by 2020.

## 2.2. REGIONAL CONTEXT

Rosemead is among the 88 cities that comprise Los Angeles County; it sits in the heart of the San Gabriel Valley, roughly 8 miles east of downtown Los Angeles, and 12 miles south of the 6,164-ft San Gabriel Peak in the San Gabriel Mountains. Since the 1990s, Rosemead has become an increasingly diverse community as large numbers of

<sup>2</sup> “Garvey Avenue from Alhambra to El Monte”, Mike Sonksen, KCET Departures Columns LA Letters, January 2015

<sup>3</sup> State of California, Department of Finance. Population Estimates for Cities, Counties, and the State, May 2014

Chinese and Vietnamese immigrants settled and established businesses. By 2010, over 60% of Rosemead's population identify themselves as Asian, with another 34% identifying as Latino. This demographic change has resulted in a transformed Garvey Avenue. Excluding chain stores, virtually all retail businesses are Asian-oriented. As stated in Rosemead's General Plan, it is vital that the Garvey Avenue Specific Plan meets the needs of the diverse population living and working along the corridor.

Garvey Avenue is an east/west corridor, with the Garvey Avenue Specific Plan portion located in southwest Rosemead adjacent to the city boundary with Monterey Park. Excluding the street and its right-of-way, the project area covers 0.14 square miles, encompassing the commercial and light industrial frontages surrounding an approximate 1.2 mile-length section of Garvey Avenue. The easternmost boundary is Charlotte Avenue, while New Avenue marks the westernmost boundary of the project area. The northern and southern boundaries of the project area vary by segment of Garvey Avenue, but Newmark Avenue stands as the southernmost boundary, and Whitmore Street is the northernmost

boundary, see Figure 2.2. Interstate 10 (I-10) is situated about half a mile north of Garvey Avenue and has entrance/exit ramps at New Avenue, Del Mar Avenue, and San Gabriel Boulevard, all providing access to Garvey Avenue. Route 60 (SR-60) is located approximately two miles south. State Route 19, better known as Rosemead Boulevard, runs north-south between the two freeways, just east of the project area boundary.

### 2.3. DEVELOPMENT PATTERNS

Rosemead is 5.2 square miles of relatively flat land with few topographical impediments to development. Rosemead's development patterns have been shaped predominantly by the post-war development resulting in low scale, low intensity development.

The I-10 freeway bisects Rosemead, creating a barrier within the city and dividing local neighborhoods. Significant nonresidential development is clustered along major avenues. These avenues include the north-south streets of Del Mar Avenue, San Gabriel Boulevard, Walnut Grove Avenue, and the east-west streets of Valley Boulevard, north of I-10, where

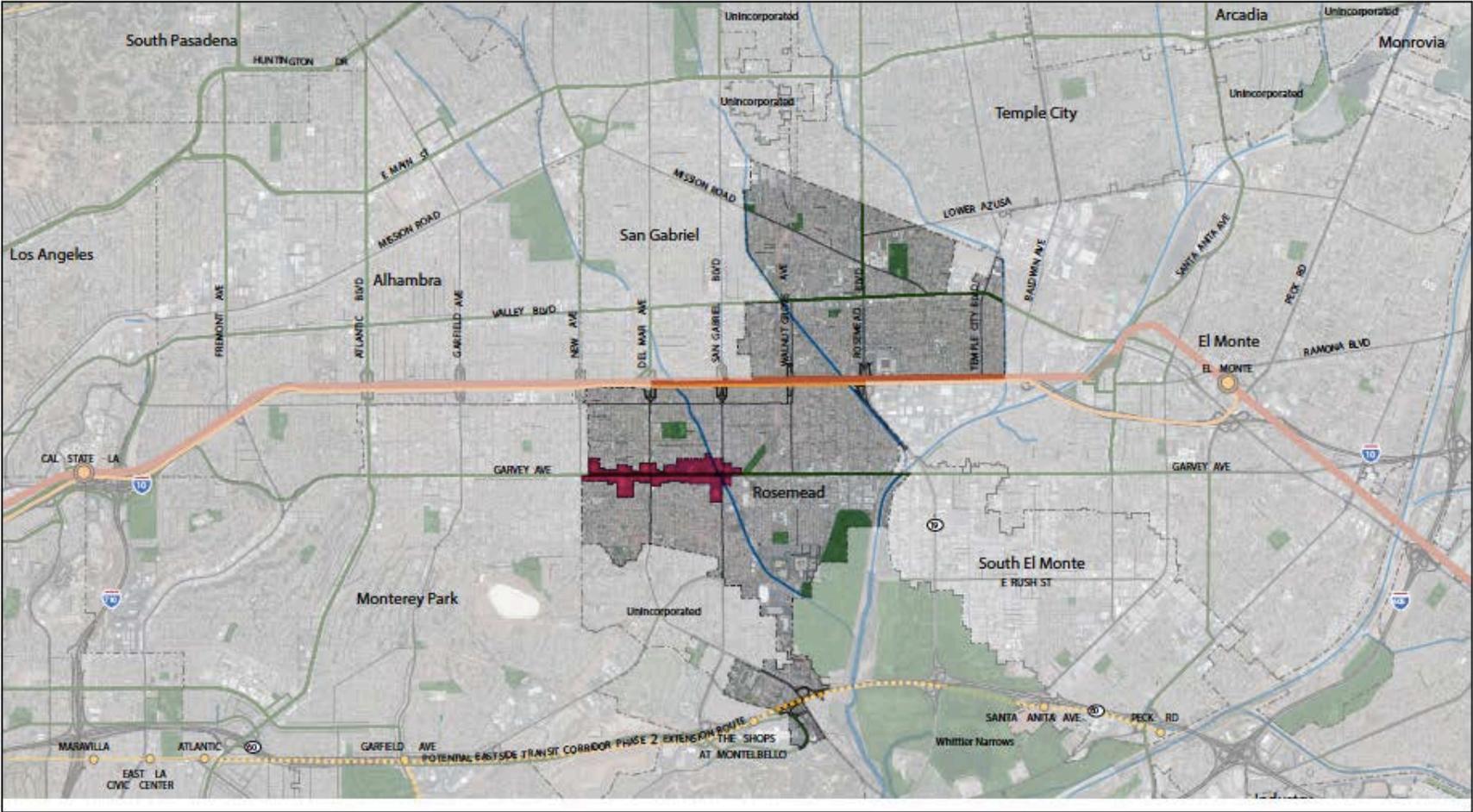
Rosemead's downtown is located, and Garvey Avenue, south of I-10, the focus of this Garvey Avenue Specific Plan.

### 2.4. LAND USE

The Garvey Avenue project area encompasses 88 acres with some 153 parcels of varying land use. The project area is bordered and surrounded by single-family or low-density multifamily housing. A majority of land uses within the project area are commercial/retail, with 84 parcels in commercial or retail use, encompassing 37 acres (42.3% of the total project area). A large number of vacant parcels make up 30.4% (nearly 27 acres) of the total project land area. The vacant land offers opportunities to develop catalytic projects to help transform Garvey Avenue into a new kind of corridor.

A large amount of land used exclusively for automobile parking presents infill development opportunities as well. The dearth of open space along the corridor highlights the need for open space and parks. The land along Alhambra Wash (and the Wash itself) is an opportunity for new green space along an underutilized water body.

Figure 2.2 Regional Context Map



Rosemead Garvey Avenue Specific Plan  
Regional Context

Legend

- City Boundary
- █ Specific Plan Area
- █ Open Space
- Creek/Channel
- ↔ Transit Route
- Metro Line + Station
- Proposed Metro Line + Station
- MetroLink + Station

0 2,000 4,000 feet

The majority of the project area is zoned for exclusively commercial uses. Nearly three fourths are zoned C-3 Medium Commercial. Another 8%, specifically the former LA Auto Auction site, is zoned as C-4 Regional Commercial. The remainder of the project area is zoned as R-2 Light Multiple Residential (9%), Automobile Parking (5%), and PD Planned Development (3%), see Figures 2.3 - 2.5.

## 2.5. URBAN DESIGN

Garvey Avenue’s built form – its street network, block pattern, parcel pattern, and building pattern – displays a range of sizes and a diversity of shapes, see Figure 2.6. However, its overall pattern is characteristic of auto-oriented “strip” corridors with single-family suburban development around it.

Wide streets with wide traffic lanes like Garvey Avenue and San Gabriel Boulevard connect well to the surrounding street grid, benefitting cars; however, narrower streets that are safer and more pleasant for pedestrians, like Whitmore and Newmark Avenues, are truncated and do not connect well.

**Figure 2.3 Land Use, 2014**



Existing land uses are primarily auto-oriented commercial.



Small mini-mall plazas like Fortune Plaza are common.



Vacant land use is common.



The Square is a major neighborhood node.

Figure 2.4 Existing Land Use

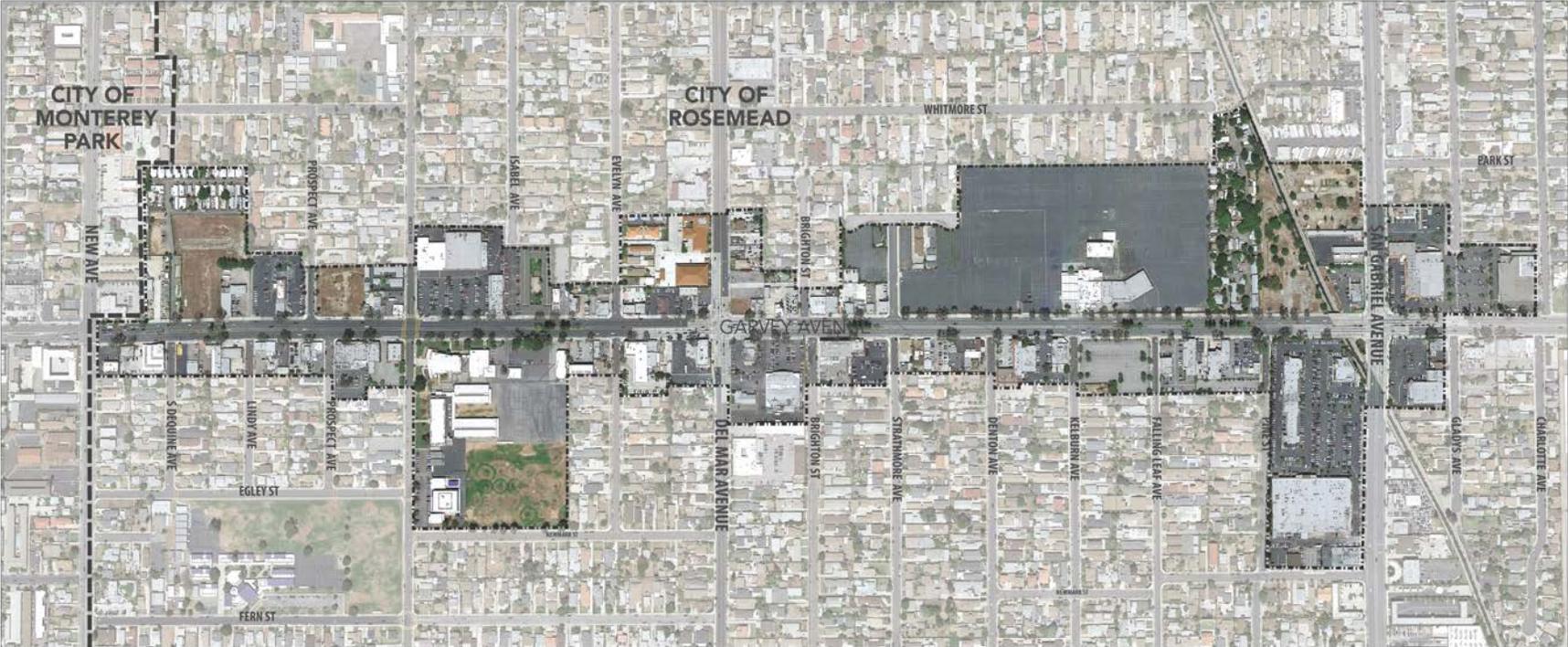
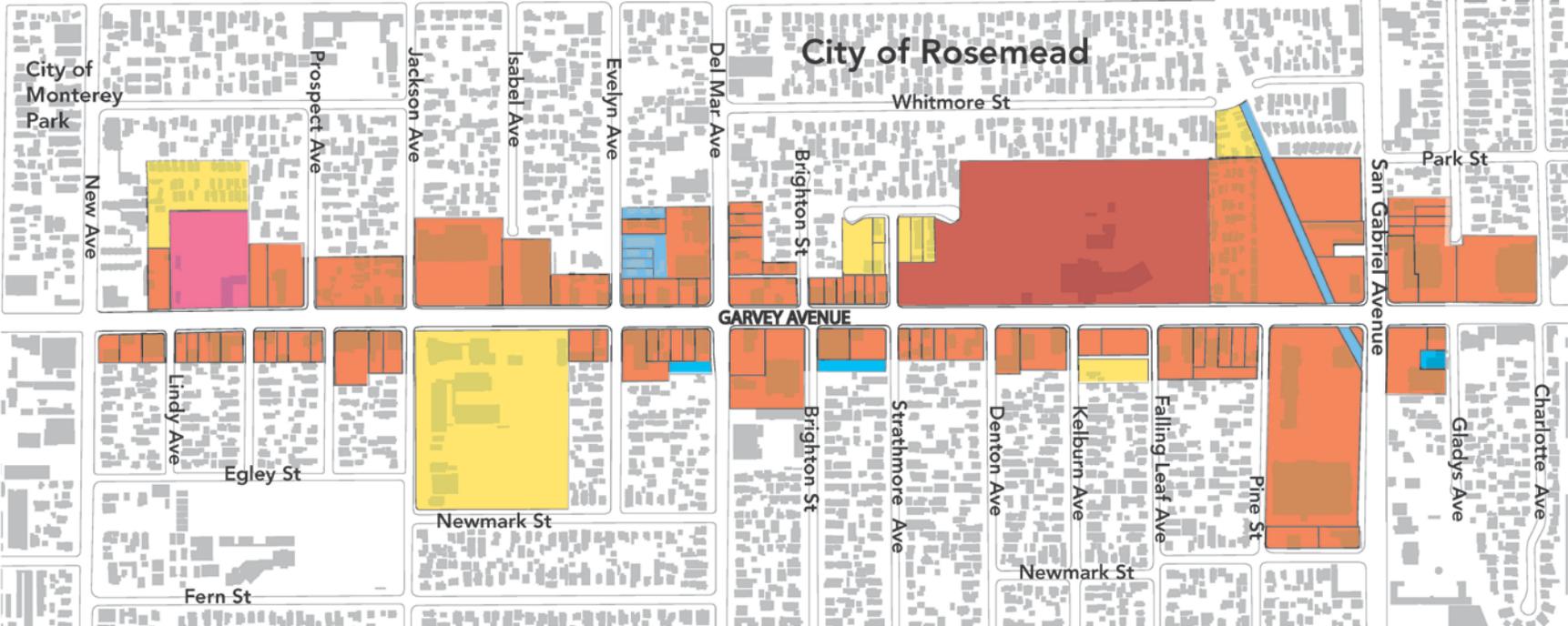


Figure 2.5 Existing Zoning



Rosemead Garvey Avenue Specific Plan

Existing Zoning

Legend

- City Boundary
- Specific Plan Area
- PD Planned Development
- P Automobile Parking
- R-2 Light Multiple Residential
- C-3 Medium Commercial
- C-4 Regional Commercial

0 400 800 1,200 feet

The wide arterials encourage fast through-traffic and, at intersections, create wide crossings that are challenging for pedestrians to navigate. Pedestrian signage is rare with more emphasis on signage for vehicles.

Block sizes are quite large, too large for easy pedestrian circulation. Few midblock alleys, pedestrian passageways, and pedestrian crosswalks are present to help pedestrians safely move in and around the area. Vacant land is cordoned off at the sidewalk with chain-link fences, creating a monotonous and intimidating pedestrian experience. No community gathering places, few vibrant public spaces, and few landmarks serve as iconic elements and provide a unique identity for the Garvey Avenue corridor. These elements are needed to make the corridor a real place.

The vacant land and surface parking lots result in a sparse building pattern, with major empty “holes” in development. Along Garvey Avenue, most development is single-story and of low building quality. Basic cinderblock or concrete garage-like auto repair shops are common, lending the corridor a gritty “light industrial” character. Buildings are often set back from the street fronted by large surface

parking lots, which results in a discontinuous edge with empty spaces and gaps along the sidewalk. These “gaps” in the “street wall,” the fabric of buildings that directly line the sidewalk, have a significant negative impact on the streetscape and pedestrian environment. Gaps in the street wall from parking lots signal to pedestrians that the area that has become inactive and vehicle-oriented. The “dead spaces” lack retail, office, or residential uses that bring activity, vibrancy, and interest to the street. Dead blocks such as these become areas that pedestrians want to pass through as quickly as possible. Curb cuts for parking lots, meanwhile, means more cars crossing the sidewalks, decreasing pedestrians’ sense of safety and comfort.

The residential areas surrounding the Garvey Avenue Specific Plan area are characterized by low-slung ranch homes. They tend to be low, wide, single-story houses with street-facing garages and paved driveways and parking areas.

Fencing is common along the property line at the sidewalk. Some special design elements help make portions of the Garvey Avenue streetscape lovely. In particular, the majestic and massive

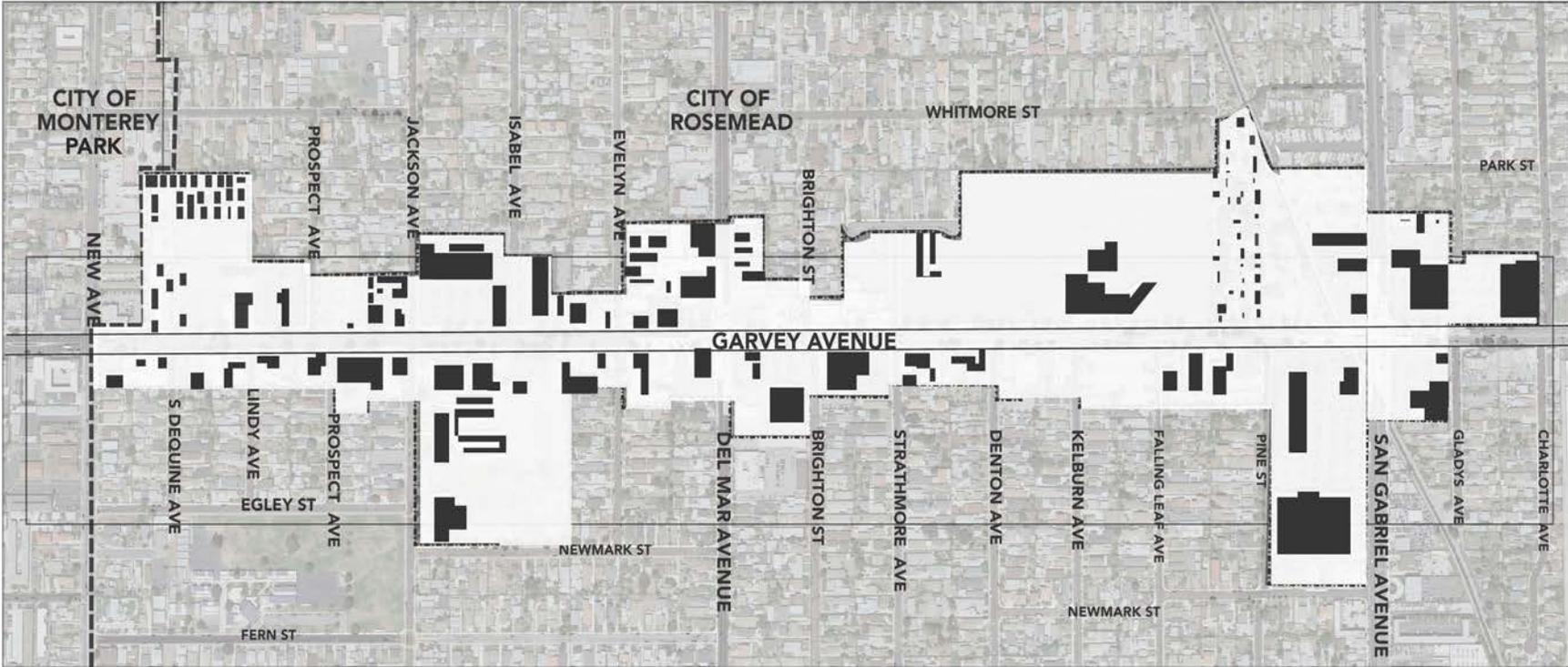
Eucalyptus trees lining portions of the sidewalks are a special and unique element that should be preserved and enhanced, when possible. They boost the streetscape and pedestrian environment, provide needed shade and air-quality benefits, and help calm traffic. In a few areas of the corridor, a planted median contributes even further to these benefits; however, the median occurs sporadically.

Most of Garvey Avenue’s 100’ right-of-way is dedicated to vehicles, with disproportionately little space dedicated to pedestrians and bicycles.



This small roundabout greens the intersection with landscape plantings.

Figure 2.6 Built Form, 2014



Rosemead Garvey Avenue Specific Plan  
Built Form

Legend

- - - City Boundary
- . - . Specific Plan
- Existing Building

0 400 800 1,200 feet



Figure 2.7 Urban Pattern, 2014



A typical residential house in the adjacent neighborhood.



Existing restaurant along Garvey Avenue.



The majestic San Gabriel Mountains dominate the view at Del Mar Avenue.



Without a median and trees, the street is less attractive and less pedestrian friendly.

## Do trees really calm traffic?

*Yes: a growing body of research supports the conclusion that trees can have a strong traffic calming effect. A Texas study showed a 46% decrease in crash rates on ten urban arterial roads across the state after trees were planted. People perceive streets with trees to be safer than streets without trees. A clear majority of drivers asked to choose which street is safer, based on images of streets with and without trees, said the street with trees is safer. The correlation between the presence of street trees and lower speeds, and the correlation between lower-speeds and streets with fewer crashes is well-established. Drivers in another study who took a simulated ride on two suburban streets, one with trees and one without, went 3 mph slower on the street with trees.*

## 2.6. MOBILITY

The Garvey Avenue corridor serves as a key connection within the city and between neighborhoods for pedestrians, bicyclists, transit users, and automobiles. Although it is designed primarily for automobiles, Garvey Avenue is used, to varying degrees, as a multi-modal corridor, see Figures 2.8 - 2.10. It is a major transit corridor with both a rapid and local bus line, and regular pedestrian and bicycle activity throughout the day. Vehicle traffic is highest in the AM and PM peak hours. The currently posted speed limit of 35 mph is lower than Walnut Grove Avenue and segments of San Gabriel Boulevard, which are posted at 40 - 45 mph depending on location.

Traffic volumes on Garvey Avenue compete with and detract from the pedestrian environment. The many surface parking lots encourage people to drive rather than walk to the corridor. The few pedestrian crossings are spaced too far apart to provide safe and easy pedestrian access to both side of the street. In addition, Garvey Avenue's long blocks also discourage pedestrian activity. At intersections, crosswalks are set by default to red for pedestrians, prioritizing vehicle

movement and forcing pedestrians to press a button to safely cross the street.

Conditions for bicyclists are also poor; there are no dedicated bicycle facilities. As such, many bicyclists ride on the sidewalk for safety. Despite the lack of facilities, bicycle ridership on Garvey Avenue is relatively high.

Rapid and local transit along Garvey Avenue competes with private vehicle traffic on the street; transit is given little priority. While bus shelters are provided on the sidewalks, there are few other amenities for transit users.

Level-of-service (LOS) vehicle analysis of nine study intersections along the corridor and in the adjacent areas shows that all the study intersections operate at good LOS values under the current conditions.

Vehicle traffic is highest in the eastern side of the corridor and lowest in the west, with roughly 1,000 vehicles per hour in the AM and PM peaks in both directions at San Gabriel Boulevard, which drops to roughly 500-700 vehicles per hour in the AM and PM peaks in both directions at New Avenue.

Figure 2.8 Existing and Planned Mobility, 2014

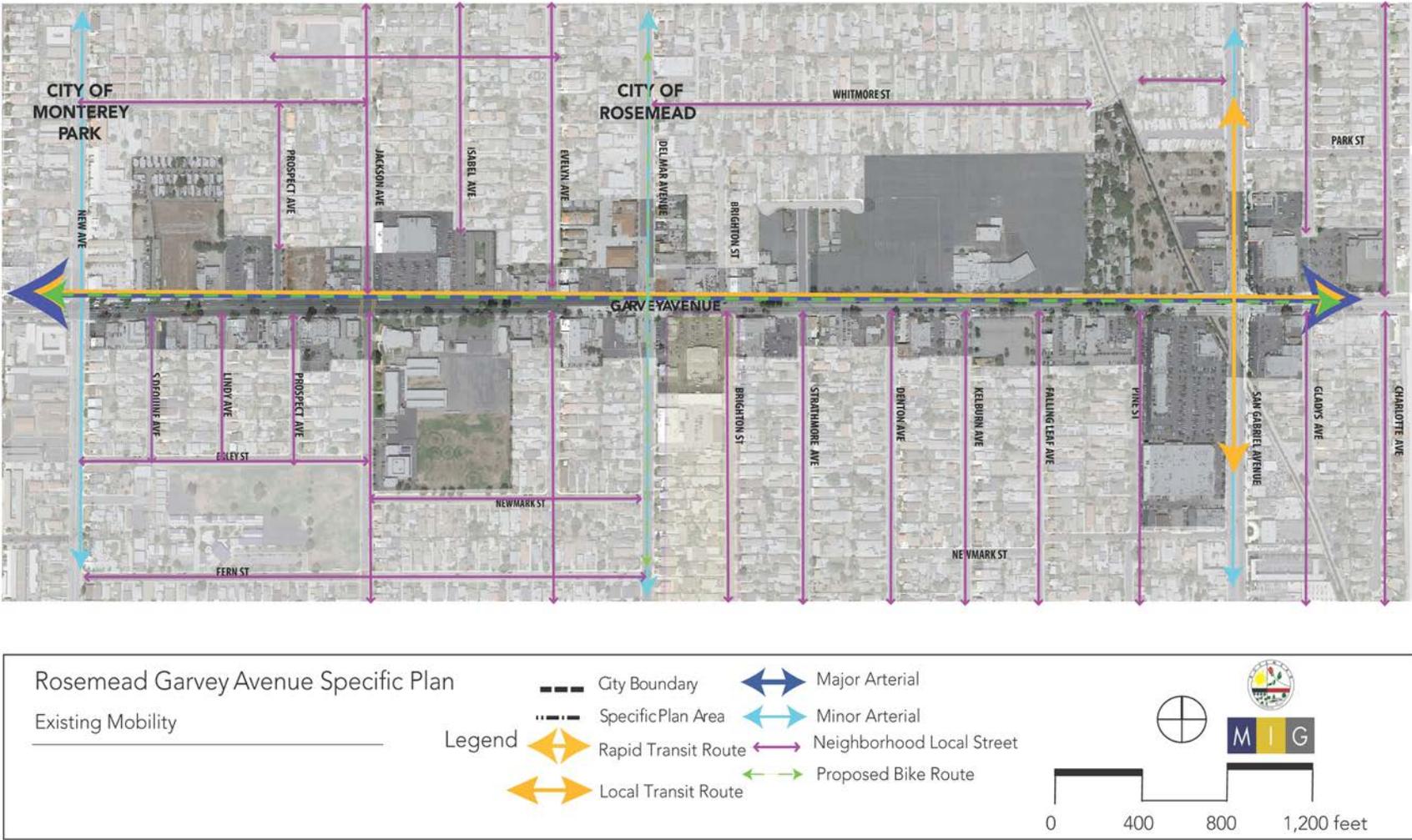
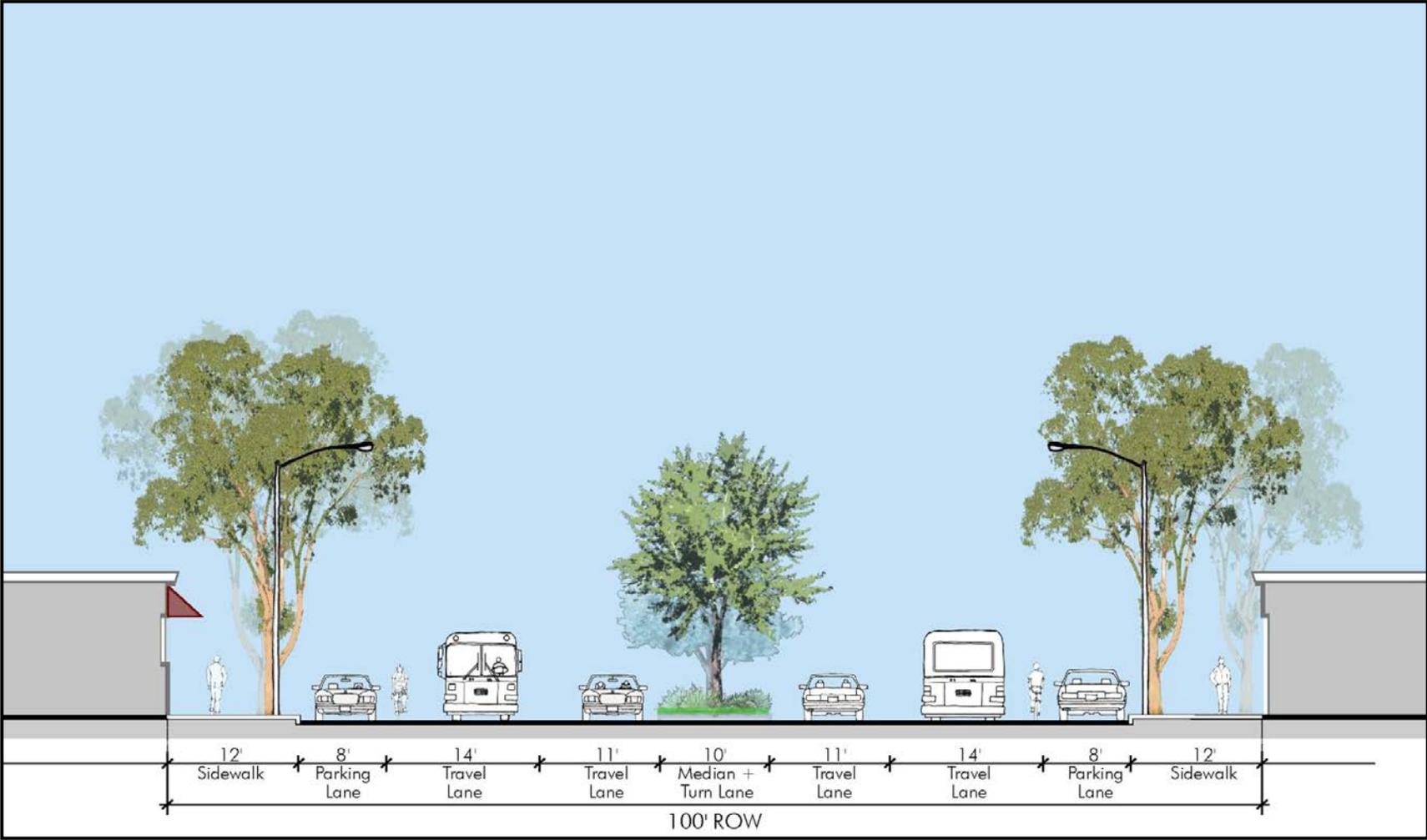


Figure 2.9 Garvey Avenue Section, 2014



**Figure 2.10 Modes of Mobility, 2014**



**Many bicyclists use the sidewalk for safety.**



**Garvey Avenue is a major transit corridor.**



**Pedestrians have few crossing opportunities.**



**Wide intersections can be dangerous and unfriendly.**

## 2.7. INFRASTRUCTURE

Key infrastructure along Garvey Avenue includes sewer, water, and stormwater management.

Some of the areas' existing sewers show capacity deficiencies, as well as maintenance and structural defects. These facilities will need to be replaced and upgraded.

Future water demands can be met until 2035 based the Golden State Water Company's (GSWC) 2010 Urban Water Management Plan - South San Gabriel System. Water lines along Garvey Avenue have the most capacity for potential increase due to the 12" main fronting the Garvey Avenue parcels.

The Garvey Avenue Specific Plan area's watershed drains into several stormwater drainage systems. The main drainage system is a box culvert that runs along Garvey Avenue to the Alhambra Wash, as well as several pipe drainage systems.

## 2.8. PUBLIC SERVICES AND FACILITIES

Garvey Avenue has a handful of public facilities within and around the Garvey Avenue Specific Plan area, see Figure 2.11. These facilities include schools and institutions, which contribute to the sense of community and provide a strong sense of place along Garvey Avenue. A major presence is Richard Garvey Intermediate School, located between Jackson and Evelyn Avenues. It is a community anchor and a significant trip generator in the neighborhood. Southwest of Garvey Intermediate School and just out of the Garvey Avenue Specific Plan area is Arlene Bitely Elementary School, another major anchor. Just north of the Garvey Avenue Specific Plan area, along Jackson Avenue, is a third public school, Emerson Elementary School.

The Buddhist Ortho-Creed Association Dharma Seal Temple is also a significant presence. Though the Dharma Seal Temple is located on Del Mar Avenue, north of the Garvey Avenue, it has a striking visual presence and is a major stakeholder along the corridor. The Association is a major landowner with a large complex encompassing a number of

parcels and several buildings. A Jehovah's Witness Church, located on Del Mar Avenue south of Garvey Avenue, is smaller yet also has a presence.



**Buddhist Ortho-Creed Association Dharma Seal Temple**

Parks and recreational facilities are notably lacking along the corridor, though there is potentially an opportunity to improve public space along Alhambra Wash, an existing open stormwater channel that cuts diagonally through the Garvey Avenue Specific Plan area. While there is a lack of parks and open space in the Garvey Avenue Specific Plan area, the active Zapopan Park is located just to the east, under the Southern California Edison power lines.

Figure 2.11 Public Facilities, 2014



Alhambra Wash



Garvey Intermediate School



AT&T Building



Zapopan Park

Power within the Garvey Avenue Specific Plan area is provided by Southern California Edison. Power lines have been undergrounded along Garvey Avenue.

Natural gas is provided by Southern California Gas, a subsidiary of Sempra Energy. The nearest major pipelines run underground along Walnut Grove Avenue (parallel to and east of San Gabriel Boulevard) and Fern Avenue (parallel to Garvey Avenue east of Walnut Grove Avenue).

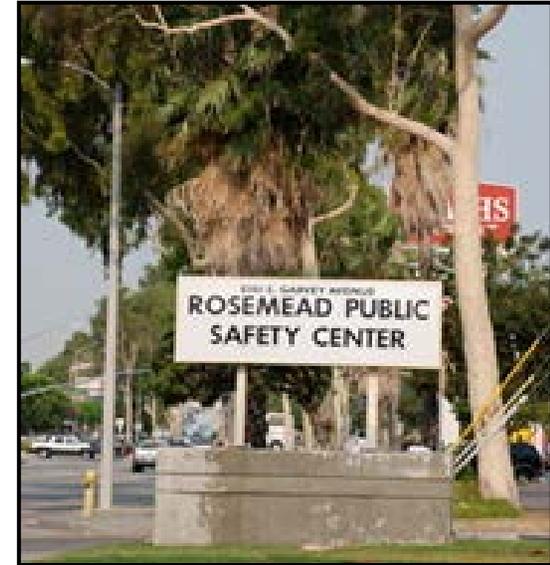
Charter Communications is the primary cable provider for Rosemead, while AT&T is the primary provider of home and business phone service and internet services.

Public library facilities are not located within walking distance of the Garvey Avenue corridor. The nearest library, Los Angeles County Public Library, Rosemead branch, is located on Valley Boulevard adjacent to the Rosemead City Hall.

Fire service and emergency medical service are provided by the Los Angeles County Fire Department, which has a local station in downtown Rosemead, on

Valley Boulevard. A second local station is located just south of the project area, on the east side of San Gabriel Boulevard.

Rosemead's police service is provided through the Los Angeles County Sheriff's Temple Station, in Temple City. Deputies assigned to Rosemead work out of Rosemead's Public Safety Center located on Garvey Avenue adjacent to Zapopan Park at Charlotte Avenue. The Public Safety Center is managed by Rosemead's City Manager's Office; it includes local offices for the Los Angeles County Sheriff, the Rosemead Special Assignment Team, the Los Angeles County Probation Officer, and community safety services.



**Rosemead Public Safety Center**



# Chapter 3

## LAND USE, ZONING, AND DEVELOPMENT STANDARDS

### IN THIS CHAPTER

- 3.1 Garvey Avenue Specific Plan Zoning Districts**
- 3.2 Development Potential**
- 3.3 Allowed, Prohibited, and Non-Conforming Uses, and Special Use Restrictions**
- 3.4 Garvey Avenue Specific Plan Design and Development Standards**

This chapter focuses on the major land use and zoning changes in the Garvey Avenue Specific Plan, as well as development standards and revitalization strategies that will be key tools in achieving the transformative vision imagined in the Garvey Avenue Specific Plan. It includes new zoning districts to encourage specific growth patterns and build on the design guidelines enumerated in the Garvey Avenue Master Plan. For ease of implementation, existing provisions of the Rosemead Municipal Code (RMC) can be assumed to apply, as directed by the Community Development Director, unless specifically stated otherwise in this Specific Plan.

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

## 3.1. GARVEY AVENUE SPECIFIC PLAN ZONING DISTRICTS

Land use designations are established to identify the allowed uses and design characteristics desired along the Garvey Avenue corridor, and to set forth the allowable density and intensity of development. This Specific Plan builds upon existing zoning regulations contained in the Rosemead Code, modifying allowed uses and development standards for each zone to facilitate development that will achieve the vision and objectives for the corridor. The limitations of the corridor’s existing zoning are clearly seen in the state of Garvey Avenue today, where the upgrading of parcels over time that would normally be expected for a developed urban area like Garvey Avenue has not occurred. The nearly 27 acres of vacant

land indicate that Garvey Avenue’s development potential has been challenged.

The Garvey Avenue Specific Plan rezones the entire Specific Plan area and establishes four new zoning districts with targeted new development standards that offer clear guidance for the types of development that can, over time, transform the Garvey Avenue corridor into a new kind of place that is vibrant, livable, and neighborhood-oriented. The new Specific Plan zoning districts are set forth in Table 3.1 and shown on the new Zoning Map in Figure 3.1. The purpose of the guidelines and standards for each of the new zoning areas are outlined on the following pages.

*A standard is objectively measured and focused. Adhering to a standard is required. Common design standards include building setbacks, heights, etc.*

*A guideline strongly suggests or recommends a desired design or form but it allows for interpretation. Adhering to a guideline is advocated but not required. Common design guidelines include color palettes, roofline shapes, etc.*

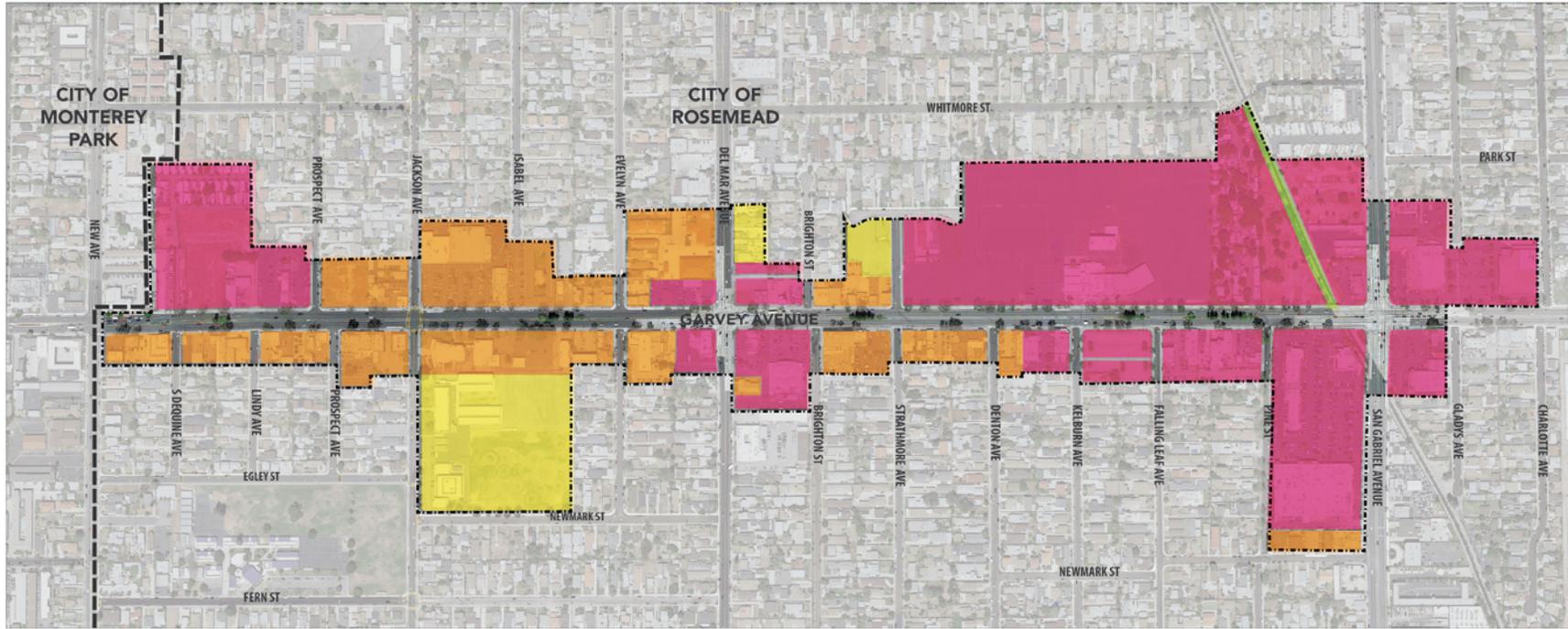
**Shall versus Should**  
*“Shall” denotes a mandatory requirement.*  
*“Should” denotes a guideline or recommendation.*

**Table 3.1 Garvey Avenue Specific Plan Zoning Districts and Land Uses**

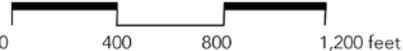
Zoning District Label	Zoning District Name	Allowed and Expected Land Uses
GSP-OS/P	Garvey Avenue Specific Plan, Open Space/Parking	Open Space, Parking
GSP-R/C	Garvey Avenue Specific Plan, Residential/Commercial	Residential, Commercial, Public, Open Space
GSP	Garvey Avenue Specific Plan	Commercial, Public, Open Space
GSP-MU	Garvey Avenue Specific Plan, Incentivized Mixed-Use	Mixed-Use, Commercial, Public, Open Space

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Figure 3.1 Zoning



**Rosemead Garvey Avenue Specific Plan Zoning**

<b>Legend</b>	<ul style="list-style-type: none"> <li> City Boundary</li> <li> Specific Plan Area</li> <li> GSP   Garvey Avenue Specific Plan</li> <li> GSP-MU   Garvey Avenue Specific Plan, Incentivized Mixed-Use</li> </ul>	<ul style="list-style-type: none"> <li> GSP-R/C   Garvey Avenue Specific Plan, Residential/Commercial</li> <li> GSP-OS/P   Garvey Avenue Specific Plan, Open Space/Parking</li> </ul>	 
			

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

## 3.1.1. GSP-OS/P GARVEY AVENUE SPECIFIC PLAN, OPEN SPACE/PARKING ZONING DISTRICT

The Garvey Avenue Specific Plan, Open Space/Parking (GSP-OS/P) zoning district allows for open space and parking development on key areas of Garvey Avenue, such as Alhambra Wash. Currently zoned for parking, the Alhambra Wash is envisioned to become a major open space amenity. Continuing to allow open space, as well as parking, will help encourage the development of much-needed open space along the Wash.

## 3.1.2. GSP-R/C GARVEY AVENUE SPECIFIC PLAN, RESIDENTIAL/COMMERCIAL ZONING DISTRICT

The Garvey Avenue Specific Plan Residential/Commercial (GSP-R/C) zoning district modifies the R-2 and C-3 zoning districts' standards to be more specific to the Plan area and to facilitate a greater opportunity for residential or commercial development. Such development in the GSP-R/C area is expected to feature designs and forms that create an appropriate pedestrian scale

along or nearby the Garvey Avenue corridor. The GSP-R/C zoning district provides transition and buffer areas between single- and multi-family residential land uses in the surrounding neighborhoods and the higher land use intensity and building scale on Garvey Avenue. The new zoning enables flexibility in development approaches while requiring high-quality design that respects and adds value to adjacent residential development.

## 3.1.3. GSP GARVEY AVENUE SPECIFIC PLAN ZONING DISTRICT

The purpose of the Garvey Avenue Specific Plan (GSP) zoning district is to facilitate and support a vibrant neighborhood commercial district accommodating a diverse range of retail, service, and office businesses, with a focus on businesses that support the needs of the local community. The GSP zoning area is intended to encourage the development of attractive retail areas where people can walk for dining, groceries, shopping, limited personal services, community and social services, and social activities and gatherings. Uses will have active retail storefronts with glass windows, open storefronts, and setbacks for outdoor dining, thus, offering

pedestrians a varied and interesting experience. Office development and some public uses are allowed on upper floors. The GSP zoning district also allows for flexible spaces where start-up and locally-owned businesses can establish and maintain small businesses. The GSP zoning district is focused on the western half of the Specific Plan area, west of Del Mar Avenue, with pockets east of Del Mar. Where new guidelines and standards have not been established, the GSP zoning code largely follows the existing zoning code for the C-3 Medium Commercial zoning area. However, stand-alone residential and mixed-use are not permitted.

## 3.1.4. GSP-MU GARVEY AVENUE SPECIFIC PLAN, INCENTIVIZED MIXED-USE ZONING DISTRICT

The Garvey Avenue Specific Plan, Incentivized Mixed-Use (GSP-MU) zoning district will transform key pockets of Garvey Avenue into active pedestrian and retail areas with a wide mix of uses to serve a variety of needs and stimulate a range of environments. The new zoning district allows for:

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

- the greatest possible flexibility in development choices, and
- a new model of development along the corridor featuring residential uses carefully integrated into buildings with active ground-floor commercial frontages. “Horizontal” mixed-use is permissible in this district, which will enable a large development site, such as the L.A. Auto Auction site, to be developed with single use buildings, provided that the overall site is developed to the mixed-use standards in GSP-MU.

All development in the GSP-MU area would be expected to feature designs and forms that create an appropriate pedestrian scale directly on the Garvey Avenue corridor, and on any internal or side streets. The GSP-MU zoning district is focused on the eastern half of the Specific Plan area, with three development nodes at the Del Mar intersection, the San Gabriel intersection, and along Garvey Avenue at the former L.A. Auto Auction site. Where new guidelines and standards

<sup>1</sup> Acres calculated in GIS from Proposed Land Use Plan PDF, 11/25/14.

have not been established, the GSP-MU zoning code follows the existing zoning code for the existing R-C MUDO Residential/Commercial Mixed-Use Development Overlay district.

## 3.2. DEVELOPMENT POTENTIAL

The Garvey Avenue Specific Plan area is roughly 88 developable acres. The land use policies associated with this Specific Plan provide the capacity for the development of approximately 1.3 million nonresidential square feet.<sup>1</sup> This development would occur as large vacant parcels are developed, as new development replaces aging properties, and as more intensely-developed commercial uses are built, either as mixed-use projects or stand-alone commercial projects. An additional estimated 1,048 dwelling units may be expected with the implementation the land use policies within this Specific Plan. This would equate to an estimated total population of approximately 2,710.<sup>2</sup> Most residential development would be built

<sup>2</sup> Population is calculated based on dwelling units and persons per multi-family household for the City of Rosemead. 2014 person per household estimate is 3.04.

almost exclusively within the GSP-MU zone, Table 3.2.

## 3.3. ALLOWED USES, NONCONFORMING USES, AND SPECIAL USE RESTRICTIONS

Allowed uses are established for each Specific Plan zoning district. Certain uses may be subject to special conditions regarding the location, operation, design, or special permitting requirements of the use. References to these provisions are made in Table 3.3.

### 3.3.1. ALLOWED USES

Allowed land uses within the Specific Plan area are listed in Table 3.3. Use listings are grouped by a general category on the basis of common function, product, or compatibility. Uses may occur anywhere within the appropriate zone. Uses may be allowed in the following ways:

- A Permitted Use (P) may be established as the primary use of a building without discretionary approval.

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

**Table 3.2 Development Potential**

Zoning Designation		Land Uses Allowed	DEVELOPMENT STANDARDS			
			Maximum Without the Provision of Community Benefits		Maximum With Provision of Community Benefits (see 3.4.3.2)	
			DU/Gross Acre	FAR	DU/Gross Acre	FAR
GSP-OS/P	Garvey Avenue Specific Plan, Open Space/Parking	Open Space, Parking	N/A	N/A	N/A	N/A
GSP-R/C	Garvey Avenue Specific Plan, Residential/Commercial	Residential, Commercial, Public, Open Space	7	0.75	30	1.0
GSP	Garvey Avenue Specific Plan	Commercial, Public, Open Space	N/A	0.75	N/A	1.0
GSP-MU	Garvey Avenue Specific Plan, Incentivized Mixed-Use	Mixed-Use, Commercial, Public, Open Space	25	1.6 MU 0.75 Com	80	3.0 MU 1.0 Com

Note: A project will use either the Mixed-Use or the stand-alone Commercial FAR on a single site. The Mixed-Use and Commercial FARs are not additive.

## LAND USE, ZONING, AND DEVELOPMENT STANDARDS

- Certain uses are allowed subject to the approval of an Administrative Use Permit (AUP), which must be approved by the Community Development Director following a review and a determination that the nature of the proposed use, at the location proposed, is subject to conditions of use and occupancy that may be set, and can be conducted without detriment to nearby properties and uses.
- Certain uses are allowed subject to the approval of a Conditional Use Permit (CUP), which must be approved by the Planning Commission following review and a determination that the nature of the proposed use, at the location proposed, is subject to conditions of use and occupancy that may be set, and can be conducted without detriment to nearby properties and uses.

CUP's are processed under Chapter 17.132 (Conditional Use Permits) of Title 17 (Zoning). Findings for CUP's are section 17.132.040 - Findings and decision authority.

A Conditional Use Permit may be issued only after a public hearing before the Planning Commission of the City upon application. The Planning Commission shall also find that the establishment, maintenance or operation of the use so applied for will not, under the circumstances of the particular case, be detrimental to the health, safety, peace, morals, comfort or general welfare of persons residing or working in the neighborhood thereof, not be detrimental or injurious to the property and improvements in the neighborhood or to the general welfare of the City. All of the following findings shall be made by the Planning Commission in conjunction with the approval of Conditional Use Permit:

- a. Approval of the application will not be or incompatible or injurious to other properties or land uses in the vicinity or create conditions materially detrimental to the public health, safety and general welfare.
- b. The use is consistent with the General Plan.
- c. The use is consistent with the provisions of the Zoning Code.

- d. Processing and approval of the permit application are in compliance with the requirements of the California Environmental Quality Act.
  - e. If development is provided for under the Conditional Use Permit, the project is consistent with the goals and objectives of the applicable standards and Design Guidelines in the overlying district.
- Certain uses are allowed subject to the approval of a Temporary Use Permit (TUP), which must be approved by the Community Development Director, in accordance with RMC Section 17.124 (Temporary Use Permits and Special Events).
  - Prohibited Use (--) refers to a use or activity that is not permitted.

The Community Development Director may determine that a proposed use not listed in Chapter 3 of the Garvey Avenue Specific Plan is permissible or conditionally permissible if the use is substantially similar to a use permitted in Chapter 3 of the Garvey Avenue Specific

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Plan and all of the following findings are made:

- a. The proposed use will be consistent with the goals, objectives, and policies of the General Plan;
- b. The characteristics of, and activities associated with, the proposed use are equivalent to those of one or more of the uses listed in the zoning districts as allowable, and are no more detrimental or intensive than the uses listed in the district for which the proposed use is determined to be consistent with; and
- c. The proposed use will meet the purpose and intent of the zoning district that is applied to the site where the proposed use is to be located.

## 3.3.2. NONCONFORMING USES

The Garvey Avenue Specific Plan area has long been developed with some light-industrial uses – including construction-related and auto repair/service businesses with outdoor operations – which the City has long encouraged to phase out due to their incompatibility with surrounding residences and the aesthetic, noise, and

traffic impacts on the corridor. Implementation of this Garvey Avenue Specific Plan will render certain light-industrial and auto-oriented commercial uses nonconforming with regard to the use provisions of this Garvey Avenue Specific Plan. To contribute to the improvement of the aesthetic quality and economic vitality along the corridor, and to ensure that land uses are consistent with the vision presented in the Garvey Avenue Specific Plan, the conditions under which legal nonconforming uses may continue is limited. Specifically, legal nonconforming uses may remain but cannot expand, change use, rebuild, and must be continually operated. If these conditions are not met, the nonconforming use is no longer a legal nonconforming use and is subject to the City of Rosemead’s Nonconforming Use Amortization Policy, RMC Section 17.72.090, except for the abandonment/discontinuation clause (RMC Section 17.72.030B). In place of the abandonment/discontinuation clause, a nonconforming use in the Garvey Avenue Specific Plan area that has been abandoned or has been discontinued for a period of 90 days shall not be reestablished and any subsequent reuse or any new use established shall conform to

the provisions of the Garvey Avenue Specific Plan.

## 3.3.3. SPECIFIC USE RESTRICTIONS

To promote active nonresidential development at the street level, office, medical, and/or dental office uses are prohibited on ground floors where facing any public street, if the uses are permitted in the zoning district. Office, medical, and/or dental office uses are permitted on ground levels in an internal configuration and/or not facing a public street, if the uses are permitted in the zoning district. Office, medical, and/or dental office uses are permitted on upper floors, if the uses are permitted within the zoning district.

## 3.3.4. ECONOMIC FEASIBILITY STUDY

An economic feasibility study, market study, or proforma analysis is required for all hotel project applications. A proforma analysis or market study is required for all mixed-use project applications.

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

**Figure 3.3 Land Use Table**

Allowed Uses Key:		GSP-OS/P	GSP-R/C	GSP	GSP-MU	Specific Regulations
P	Permitted Use					
CUP	Conditional Use Permit					
AUP	Required					
--	Administrative Use Permit					
TUP	Required					
	Use Not Permitted					
	Temporary Use					
<b>RESIDENTIAL</b>						
Single-Family Dwellings		--	P	--	--	
Two-Family Dwellings (Duplex)		--	P	--	--	
Multi-Family Dwellings		--	P	--	--	
Multi-Family Dwellings (as part of a Mixed-Use Project)		--	--	--	P	A mixed-use project application must be accompanied by an economic feasibility study, market study, or proforma analysis prepared by a reputable economic or marketing professional or firm. Economic feasibility study, market study, or proforma analysis findings must support the proposed mixed-use project, the project's land use mix components, and the extent of the land use. City of Rosemead staff will evaluate and determine the marketing professional/firm credentials to prepare such study. The Community Benefit Incentive is not applicable to these economic feasibility studies, proforma analyses, or market studies.
Artist Live/Work Space		--	AUP	--	P	
Single-Room Occupancy (as defined by §17.30.200)		--	CUP	--	CUP	See RMC Section 17.30.200 (Single Room Occupancy)
Residential accessory uses and structures		--	P	--	P	See RMC Section 17.32 (Accessory Structures) and Section 17.12.030
Home Occupations, including Cottage Food Operations (Accessory)		--	P	--	P	

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Allowed Uses Key:	GSP-OS/P	GSP-R/C	GSP	GSP-MU	Specific Regulations
<b>CARE USES</b>					
Transitional Housing	--	P	--	--	
Supportive Housing	--	P	--	--	
Child Care Home, Large Family (9 to 14)	--	AUP	--	--	See RMC Section 17.30.160 (Large and Small Family Child Day Care Home Facilities)
Child Care Home, Small Family (8 or fewer)	--	P	--	--	See RMC Section 17.30.160 (Large and Small Family Child Day Care Home Facilities)
Residential Care Facilities (6 or fewer)	--	P	--	--	
Residential Care Facilities (7 or more)	--	CUP	--	--	
<b>PUBLIC AND CIVIC</b>					
Colleges and Universities	--	--	--	P	
Cultural Institutions	CUP	CUP	CUP	CUP	
Park and Recreation Facilities	P	P	P	P	For lighted facilities, see RMC Section 17.68.060
Places of Religious Assembly	--	CUP	CUP	CUP	
Public Utility Facilities	AUP	AUP	AUP	AUP	
Telecommunication Facilities/Wireless Telecommunication Facilities	CUP	CUP	CUP	CUP	
Educational Institution (Private)	--	CUP	CUP	CUP	
Community Garden	P	P	P	P	
Open Space, Public	P	--	--	--	
Hiking Trails, Public	P	--	--	--	
<b>COMMERCIAL</b>					
Animal Grooming Services	--	P	P	P	No overnight boarding of animals allowed.
Veterinary	--	P	P	P	

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Allowed Uses Key:	GSP-OS/P	GSP-R/C	GSP	GSP-MU	Specific Regulations
Drive-Through Businesses	--	--	AUP	--	See RMC Section 17.30.110
Eating and Drinking Establishments: No Alcohol Beverage Sales	--	P	P	P	
Eating and Drinking Establishments: With "On Sale" ABC License	--	--	CUP	CUP	See RMC Section 17.30.040 (Alcohol Beverage Sales)  A regional or national chain restaurant larger than 6,000 s.f. is permitted to serve alcohol without a CUP, provided that a valid license from the California Department of Alcoholic Beverage Control is obtained.
Sidewalk Dining (accessory use to eating and drinking establishments)	--	P	P	P	1) Location Requirements:  (a) A sidewalk dining, where permitted, may be located on the public right-of-way adjacent to the restaurant serving the sidewalk dining. Approval for sidewalk dining may be granted after review of the application by appropriate City departments and issuance of an encroachment permit or license agreement.  (b) All sidewalk dining shall leave clear space for pedestrian movement between the outer edge of the dining and the curb line. Sidewalk dining located at street intersections shall provide a 15' clear space radial to the corner. If pedestrian traffic is especially heavy, the Public Works Director may require additional clear space to ensure adequate room for pedestrian movements.  (c) No sidewalk dining shall be located within 15' of a bus stop or bus shelter.

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Allowed Uses Key:	GSP-OS/P	GSP-R/C	GSP	GSP-MU	Specific Regulations
					<p>2) Physical Design Requirements:</p> <ul style="list-style-type: none"> <li>(a) All furnishings of sidewalk dining including, but not limited to, tables, chairs and decorative accessories, shall be readily movable.</li> <li>(b) No part of sidewalk dining may be permanently attached to public space. The person to whom the business license for the dining is issued shall repair any damage done by the dining to public property.</li> <li>(c) When a sidewalk dining or the adjacent restaurant is occupied, no exit door shall be locked, bolted, or otherwise fastened or obstructed so that the door cannot be opened from the inside.</li> <li>(d) Chairs and tables shall be arranged so as to provide for clear access to an exit. No part of an aisle shall be used in any way that will obstruct its use as an exit or that will constitute a hazardous condition.</li> <li>(e) Sidewalk dining shall not be arranged so as to restrict the use of emergency exits, fire escapes on adjacent buildings and access to fire hydrants.</li> <li>(f) Freestanding or table mounted shade umbrellas shall be kept in good repair and may be used only where space permits.</li> <li>(g) Freestanding heating or misting equipment may be used only where space permits.</li> </ul>

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Allowed Uses Key:	GSP-OS/P	GSP-R/C	GSP	GSP-MU	Specific Regulations
					<ul style="list-style-type: none"> <li>(h) Freestanding lamps are not permitted. Flashing or moving lights are not permitted. Table candles may be used. Electric wiring shall not be placed in pedestrian areas.</li> <li>(i) Awnings shall be kept in good repair.</li> <li>(j) Seating and accessories and other components of the sidewalk dining shall be maintained in a neat and safe manner.</li> <li>(k) The height of a railing, fence, or planter (including plantings) used to establish boundaries of seating areas shall be at least 24” in height but not higher than 36”. Planters and/or plantings shall be maintained in a neat and orderly manner, and shall not encroach past the approved sidewalk dining area.</li> <li>(l) Plank-style picnic tables with bench seating are not permitted.</li> </ul> <p>3) Dining Operation Requirements:</p> <ul style="list-style-type: none"> <li>(a) Sidewalk dining shall be operated and maintained in accordance with the applicant's building plans approved by the Community Development Director and the Public Works Director.</li> <li>(b) The owner(s) shall be responsible for the removal of all wrappings, litter, and food, and shall provide thorough and sanitary cleaning for sidewalk dining area and the immediate surroundings of such area each</li> </ul>

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Allowed Uses Key:	GSP-OS/P	GSP-R/C	GSP	GSP-MU	Specific Regulations
					<p>day after the eating and drinking establishment closes.</p> <p>(c) Sidewalk dining shall not operate earlier than 8:00 a.m. or later than 12:00 a.m. (midnight).</p> <p>(d) If alcoholic beverages are permitted in the sidewalk dining area by a Conditional Use Permit (CUP), a landscape separation shall be required to prevent the passing and/or carrying of alcoholic beverages out of the sidewalk area and signs noting such requirement shall be posted in conspicuous locations.</p>
Entertainment and Spectator Sports: 1-149 seats or under 15,000 s.f.	--	--	CUP	CUP	
Entertainment and Spectator Sports: 150+ seats or over 15,000 s.f.	--	--	--	CUP	
Financial Services	--	P	P	P	
Food and Beverage Retail Sales	--	P	P	P	See RMC Section 17.30.040 (Alcohol Beverage Sales)
Hotel (50+ guest rooms)	--	--	CUP	CUP	<p>See RMC Section 17.30.130</p> <p>A hotel project application must be accompanied by an economic feasibility study, market study, or proforma analysis prepared by a reputable economic or marketing professional or firm. The analysis' or study's findings must support the proposed hotel project. City of Rosemead staff will evaluate and determine the marketing professional/firm credentials to prepare such study. The Community Benefit Incentive is not applicable to this market</p>

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Allowed Uses Key:	GSP-OS/P	GSP-R/C	GSP	GSP-MU	Specific Regulations
					study, economic feasibility study, nor proforma analysis.  Two parking spaces designed for and identified as “tour bus parking only” are required to be provided by a hotel facility in the GSP-MU zone.
Health/Fitness Club (Small)	--	P	P	P	
Health/Fitness Club (Large)	--	AUP	AUP	AUP	
Medical Office	--	--	P	P	Prohibited medical services: extended care, specifically, convalescent health or nursing care; medical or custodial care provided in cases of prolonged illness or rehabilitation.
Office	--	P	P	P	
Parking, Commercial (Non-accessory)	P	AUP	AUP	--	
Tour Bus Parking (Accessory - Hotel only)	--	--	--	P	Two parking spaces designed for and identified as “tour bus parking only” are required to be provided by a hotel facility in the GSP-MU zone.
Personal Service (General and Studio)	--	P	P	P	
Repair Service (including bicycles, excluding automotive)	--	P	P	P	
Retail Sales (General)	--	P	P	P	Prohibited Retail Uses: building materials and supplies sales, firearms sales, liquor stores, second hand stores, and pawn stores.
Tutoring Services (Small)	--	P	P	P	
Tutoring Services (Large)	--	AUP	AUP	AUP	

The required economic feasibility study, proforma analysis, or market study is not eligible for the Community Benefit Incentive program. The economic feasibility study, market study, or proforma analysis must affirm or support the land use mix and the extent of the proposed land uses. An economic feasibility study, market study, or proforma analysis must be prepared by a reputable economic or market professional or firm. The City of Rosemead staff will affirm the economic or market professional or firm's credentials to prepare such study, and affirm the study's findings.

### 3.4. GARVEY AVENUE SPECIFIC PLAN DESIGN AND DEVELOPMENT GUIDELINES AND STANDARDS

The Garvey Avenue Specific Plan design and development guidelines and standards provide specific policies for how parcels and buildings shall be developed, such as setbacks and parking requirements, or height and density limits (Figures 3.2 and 3.3). They are intended to supplement the development standards in Rosemead's General Plan, as well as the design

guidelines outlined in the Garvey Avenue Master Plan and in Rosemead's Mixed-Use Design Guidelines, both of which are incorporated by reference. These documents specifically addressed many design guidelines important to this Specific Plan, including but not limited to those that relate to the public realm and pedestrian environment, building and storefront design, parking, and utilities. The design standards and guidelines of this Garvey Avenue Specific Plan are largely consistent with those in the Garvey Avenue Master Plan and Rosemead's Mixed-Use Guidelines, and, likewise, largely share the same goals as those in the Garvey Avenue Master Plan and Rosemead's Mixed-Use Guidelines. These goals include:

- a. Upgrading the image and appeal of Garvey Avenue as a safe, attractive, and high-quality commercial environment;
- b. Encouraging high quality infill and mixed-use redevelopment of vacant lots and underutilized properties to their highest and best use, whether commercial, residential, office, entertainment, or open space;

- c. Activating the street and enhancing the pedestrian environment and scale;
- d. Ensuring compatibility between adjacent uses, especially single-family residential and other mixed-use projects;
- e. Inviting and supporting transit and active transportation;
- f. Crafting parking requirements that balance parking needs with updated standards that give flexibility to developers, manage parking as efficiently as possible, and minimize the negative impacts of parking on the neighborhood; and
- g. Integrating high-quality landscape and streetscape design that is consistent throughout the corridor.

The Garvey Avenue Specific Plan design and development standards provide the foundation for a distinct corridor identity. Through the use of these custom-tailored design guidelines and development standards, Rosemead can leverage successful existing land uses, redevelop underutilized sites to their highest and best

use, and capitalize on once-in-a-lifetime opportunities to redevelop several large and catalytic vacant sites, such as the former L.A. Auto Auction site.

### 3.4.1. USE OF THE DESIGN AND DEVELOPMENT

All subdivisions; new land uses and structures; and substantial rehabilitation, alterations, and/or remodeling of existing land uses and structures shall be designed, constructed, and established in compliance with the requirements of this Section, in addition to the applicable standards in Title 17 (Zoning) of the Rosemead Municipal Code *unless explicitly defined, stated, or delineated otherwise in this Garvey Avenue Specific Plan*. Principal and accessory structures shall meet the same development standards unless otherwise modified in this Garvey Avenue Specific Plan.

**Figure 3.2 Specific Plan Vision**



**The Specific Plan seeks to create vibrant new places to walk and shop.**



**Gathering places and community spaces will activate the public realm.**



**A green streetscape and distinct branding will identify Garvey Avenue as a special place.**



**Bicyclists, pedestrians, and drivers will integrate seamlessly into a strengthened network.**

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

## Figure 3.3 Additional Specific Plan Vision



Modulated storefronts with signage complement a green pedestrian realm.



Outdoor dining invites pedestrians to shop and eat.



An enhanced pedestrian realm benefits all community members.



A consistent building edge with varied modulation and no gaps.

## LAND USE, ZONING, AND DEVELOPMENT STANDARDS

With regard to the substantial rehabilitation, alterations, and/or remodeling of existing land uses and structures, whenever such activity involves removal of more than 50% of the exterior walls of a structure (this shall mean the removal of the following examples of building materials: wood, siding of any type, stucco, windows, framing materials, and will not require the demolition of the structural elements/materials of the wall(s) to be considered within the 50%), the replacement construction is considered to constitute a new structure, and such rehabilitation, alteration, and/or remodeling activity shall comply with all development standards set forth in this Garvey Avenue Specific Plan.

The Garvey Avenue Specific Plan design standards and guidelines should be used by landowners, developers, tenants, and their consultants, such as architects, who propose any alteration, addition, constructions and/or development projects within the Garvey Avenue Specific Plan area. City staff should use the Plan to review projects for 1) compliance with the design standards, and 2) compliance with the intent of the design guidelines. Individuals and entities proposing projects

within the Garvey Avenue Specific Plan area should review and understand these standards and guidelines before initiating the design and development process. To facilitate project approvals, questions regarding the design standards and guidelines, as well as other development-related questions, should be discussed with the Community Development Director or designee as early in the development process as possible.

Individuals and entities proposing projects should use these design standards and guidelines at each project stage to shape concepts and designs to realize compatible architecture and urban design that meets City of Rosemead requirements and expectations. City staff and others use these standards and guidelines to understand proposed projects in relationship to approved objectives, goals, standards, and guidelines.

The Garvey Avenue Specific Plan development standards are outlined in Table 3.4. A map of height limits for development along Garvey Avenue is shown in Figure 3.4.

### 3.4.2. HEIGHT EXCEPTION

A 5' bonus may be granted by the Planning Commission, upon recommendation from the Community Development Director and in accordance with the requirements of Section 17.28.030(D)(13)(a)(2)(a), if it is determined that the additional height would provide unique architectural elements that would enhance the project overall.

### 3.4.3. COMMUNITY BENEFITS INCENTIVES

Community Benefit Incentives are provided to allow developer and property owners to increase the development potential if community benefits are identified as part of the development application, constructed as part of the project development, and operated in perpetuity. Restrictions and/or covenants are required to be recorded on the property to ensure the benefits or amenities provided to earn the Community Benefit Incentive are maintained in perpetuity. Benefits can be obtained in two ways – Affordable Housing and Senior Housing (See 3.4.3.1) or Garvey Avenue Community Benefit Program (See 3.4.3.2).

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

## Table 3.4 Development Standards

Specific Plan Standards	GSP-OS/P	GSP-R/C	GSP	GSP-MU	Comments
<b>DEVELOPMENT INTENSITY AND NEIGHBORHOOD COMPATIBILITY</b>					
<b>Minimum Lot Size</b>	See RMC Section 17.08.050 regarding lot area and dimension requirements for direction on an undeveloped, substandard, or nonconforming lot.				
	None	6,500 s.f.	5,000 sq.	Mixed-Use 10,000 s.f. Other 5,000 s.f.	
<b>Minimum Lot Width</b>	50'	70'	70'	100'	
<b>Maximum Density Without the Provision of Community Benefits</b>	N/A	7 dwelling units/gross acre	N/A	25 dwelling units/gross acre	
<b>Maximum Density With the Provision of Community Benefits</b>	N/A	25 dwelling units/gross acre	N/A	80 dwelling units/gross acre	
<b>Minimum Unit Size</b>					
<b>Studio</b>	N/A	600 s.f.	N/A	600 s.f.	
<b>One-Bedroom</b>	N/A	600 s.f.	N/A	600 s.f.	
<b>Two-Bedroom</b>	N/A	800 s.f.	N/A	800 s.f.	
<b>Each Additional Bedroom</b>		An additional 200 s.f./ bedroom	N/A	An additional 200 s.f./ bedroom	
<b>COMMERCIAL DEVELOPMENT INTENSITY</b>					
<b>Floor Area Ratio (FAR) Without the Provision of Community Benefits</b>	None	0.75 maximum	0.75 maximum	Commercial: 0.75 maximum Mixed-Use: 1.6 maximum	
<b>FAR With the Provision of Community Benefits</b>	None	1.0 maximum	1.0 maximum	Commercial: 1.0 maximum Mixed-Use: 3.0 maximum	
<b>Required Floor Area of the Ground Floor Space in a Vertical Mixed-Use Building located along Garvey Avenue</b>	N/A	N/A	N/A	Lots with 50' or less of street frontage: 800 s.f., minimum  Lots with 51' or more of street frontage: 20% of the lot area, minimum.  A minimum of 20% of the building footprint shall be dedicated to ground floor space.	
<b>REQUIRED MIXED-USE LAND USE SPLIT</b>					
<b>Floor-Area Land Use Mix</b>	N/A	N/A	N/A	65% Residential Use and 35% Nonresidential Use (Mixed-Use Development Only)	See Table 3.5 Community Benefit Amenities and Associate Points

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Specific Plan Standards	GSP-OS/P	GSP-R/C	GSP	GSP-MU	Comments
<b>BUILDING HEIGHT AND FORM</b>					
<b>Maximum Height</b>	Maximum height is calculated to the top of roofline or roof structures including but not limited to elevator housing, stairways, tanks, ventilating fans, roof signs, etc.				
	35'	35'	75'	75'	
<b>Height Exception</b>	An additional 5' beyond the height limit is allowed for unique architectural elements as determined by the Community Development Director.				Section 3.4.2 of this Specific Plan
<b>Maximum Building Length</b>	Building façade lengths may not exceed 300'.				
<b>BUILDING RELATIONSHIP TO STREET</b>					
<b>Minimum Building Placement on Lot Frontage</b>	Minimum lot frontage that must be developed by a building				Section 3.4.4 of this Specific Plan
	None	60%	60%	Less than 1.00 acre site – 60%  1.00 acre site to 6.99 acre site – 60% <ul style="list-style-type: none"> <li>• The 60% requirement may be satisfied with: building placement on the property line (nonresidential) or setback line (residential) for 60% of the lot width,</li> <li style="text-align: center;">OR</li> <li>• Building placement on the property line (nonresidential) or setback line (residential) equal to a minimum of 25% of the lot frontage, and</li> <li>• Vertical feature placement on the property line (nonresidential) or setback line (residential) equal to a maximum of 35% of the lot frontage. Vertical features satisfying this requirement are: (1) highly landscaped decorative wall, which screens parking area from view of the public right-of-way, or (2) a highly landscaped public plaza/public amenity space incorporating a decorative wall. The vertical feature's and/or decorative wall's design and placement</li> </ul>	All decorative walls shall be a maximum of 3' in height.

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Specific Plan Standards	GSP-OS/P	GSP-R/C	GSP	GSP-MU	Comments
				must be approved by the Community Development Director  7.00 acre or greater site – 60%	
<b>Ground Floor Height</b>	14' minimum	10' minimum	14' minimum	Nonresidential: 14' minimum Residential: 10' minimum	Section 3.4.8 of this Specific Plan
<b>Elevation Above Street Level</b>					
<b>Ground Floor Living Space</b>	N/A	2' minimum 3' maximum	N/A	2' minimum 3' maximum	Section 3.4.8 of this Specific Plan
<b>Ground Floor Nonresidential</b>	0' minimum 2' maximum				
<b>Nonresidential Façade Height at or near Street Frontage</b>	Minimum height for nonresidential building façade at or near the street frontage, measured to the top of the façade. For single story buildings, a false front or parapet should be used to achieve this minimum height. Where exterior frontage height varies along the building frontage, the minimum height shall be considered to be the average height of the building frontage.				Section 3.4.8 of this Specific Plan and Figure 3.9
	N/A	25' minimum	25' minimum	25' minimum	
<b>GROUND FLOOR BUILDING DESIGN</b>					
<b>Ground Floor Blank Walls</b>	The amount of the ground level wall area directly visible from the street allowed to be left blank. The ground level wall area is defined as that portion of the building elevation from grade to a height of 9'.  25% maximum				Figure 3.8 of this Specific Plan
<b>Ground Floor Wall Glazing</b>	The area of ground level wall area that must be glazed with clear glass display windows and entries.  Nonresidential: 50% minimum Residential: 40% minimum				Section 3.4.5 and 3.4.8 of this Specific Plan and Figure 3.7
<b>SETBACKS FOR LIGHT, AIR, AND PRIVACY</b>					
<b>Front</b>	N/A	Nonresidential: No minimum Residential: 10'	No minimum	Nonresidential: No minimum Ground Floor Residential: 10'	
<b>Side – Adjacent to Nonresidential Use or Zoning District Other Than R-1 and R-2</b>	N/A	5' minimum	No minimum unless required by Community Development Director, Public Works Director, City Manager or his/her designee, or other reviewing agency. In such	No minimum unless required by Community Development Director, Public Works Director, City Manager or his/her designee, or other reviewing agency. In such a case, the minimum setback will be determined by the Community Development Director, City Engineer, City Manager, or other reviewing agency.	

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Specific Plan Standards	GSP-OS/P	GSP-R/C	GSP	GSP-MU	Comments
			a case, the minimum setback will be determined by the Community Development Director, City Engineer, City Manager, or other reviewing agency.		
<b>Side – Adjacent to Existing Residential, School, or Park Use</b>	N/A	10' minimum			
<b>Side – Adjacent to R-1 or R-2 Zoning District</b>	All residential, nonresidential, and mixed-use developments shall have a side variable height when abutting R-1 or R-2 zone. This specifies a setback minimum of 10' from the property line, with the height increasing at a 60 degree angle from that point.				
	N/A	10' minimum			
<b>Rear</b>	N/A	Nonresidential: 20' minimum if abutting residential, otherwise no minimum required  Residential: The lesser of 20' or 20% of lot depth	20' minimum if abutting existing residential use, school, or park; otherwise, no minimum required	20' minimum if abutting existing residential use, school, or park, otherwise no minimum required	
<b>Rear – Adjacent to R-1 or R-2 Zoning Districts</b>	All residential, commercial, and mixed-use developments shall have a rear variable height when abutting R-1 or R-2 zones. This specifies a setback minimum of 25' from the property line, increasing at a 60 degree angle from that point.				
<b>PEDESTRIAN-FRIENDLY AUTO CIRCULATION &amp; ACCESS</b>					
<b>Access Driveway Width</b>	One Way: 14' minimum, 20' maximum Two Way: 24' minimum, 30' maximum				
<b>Curb Cuts</b>	1 curb cut/lot, if lot has less than 300' of lot frontage. 1 curb cut/300' of lot frontage, if lot frontage is greater than or equal to 300', unless approved by Community Development Director and City Engineer, or City Manager. Example: 450' lot frontage is allowed 1 curb cut; 600' lot frontage is allowed 2 curb cuts.				Section 3.4.9 of this Specific Plan and Figure 3.11
<b>Frontage Dedicated to Parking and/or Driveways</b>	30% of lot frontage maximum			20% of lot frontage maximum	
<b>PARKING</b>					
<b>Minimum Nonresidential Vehicle Parking</b>					
<b>Restaurant</b>	N/A	N/A	1 standard sized parking space/300 s.f.	Restaurants with floor area less than 2,500 s.f.: 1 standard sized parking space per 400 s.f.	All stalls shall be double

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Specific Plan Standards	GSP-OS/P	GSP-R/C	GSP	GSP-MU	Comments
				<p>Restaurants with floor area greater than or equal to 2,500 s.f.: 1 standard sized parking space per 200 s.f.</p> <p>Outdoor seating area utilized in conjunction with an approved eating and/or drinking establishment shall not count towards calculations for off-street parking requirements. However, if the outdoor area is utilized in conjunction with nonresidential use, other than eating and/or drinking establishment, such outdoor area shall count towards calculations for off-street parking requirements.</p>	striped and standard sized
<b>Hotel</b>	N/A	N/A	N/A	<p>1 standard sized parking space per living or sleeping unit plus:</p> <p>1 standard sized parking space for every 50 s.f. of dance hall, exhibition space, meeting room;</p> <p>1 standard sized parking space per 300 s.f. for restaurant space; and</p> <p>1 standard sized parking space per 400 sq. of all other uses within the hotel</p>	All stalls shall be double striped and standard sized
<b>Nonresidential other than Restaurant and Hotel</b>	N/A	1 standard sized parking space/300 s.f.	1 standard sized parking space/300 s.f.	1 standard sized parking space/400 s.f.	All stalls shall be double striped and standard sized. The required parking stalls may include up to 25% of the total stalls as compact parking
<b>Minimum Residential Vehicle Parking</b>					
<b>Residential (includes guest parking)</b>	N/A	For residential developments, the project shall provide no less than 1.0 standard sized parking space/dwelling unit.	N/A	For residential developments, the project shall provide no less than 1.0 standard sized parking space/dwelling unit.	All stalls shall be double striped and standard sized. The required parking stalls

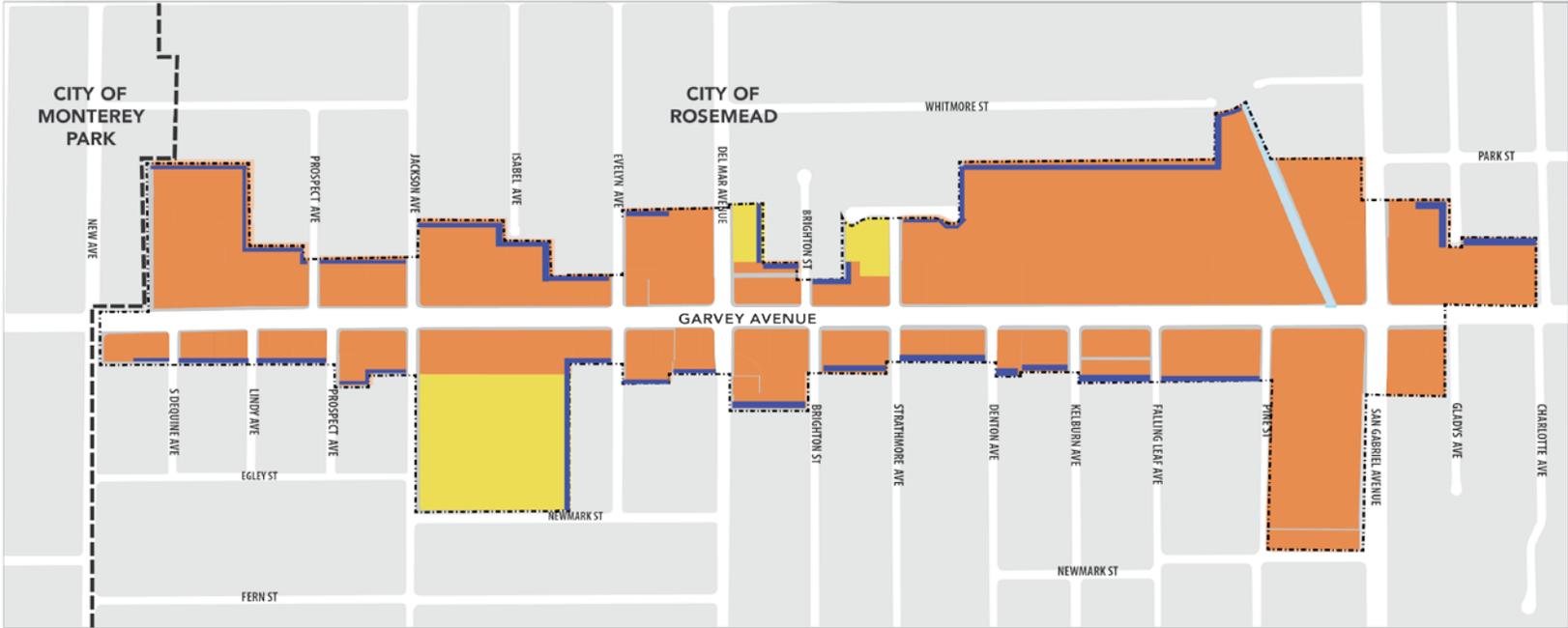
# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Specific Plan Standards	GSP-OS/P	GSP-R/C	GSP	GSP-MU	Comments
		In addition to the residential spaces described above, 0.5 standard sized parking space/dwelling unit is required guest parking.  Parking provided for residential uses must be covered and secure. Guest parking may be uncovered.		In addition to the residential spaces described above, 0.5 standard sized parking space/dwelling unit is required guest parking.  Parking provided for residential uses or the residential component of a mixed-use structure must be covered and secure. Guest parking may be uncovered.	may include up to 25% of the total stalls as compact parking.
<b>Minimum Bicycle Parking</b>					
<b>Bicycle Parking</b>	See RMC Section 12.32.030 (B).	See RMC Section 12.32.030 (B).  Bicycle parking spaces provided for residential use must be covered, secure, and located separately from bicycle parking spaces provided for nonresidential uses.	See RMC Section 12.32.030 (B).	See RMC Section 17.28.030(D)(2)(c).  Bicycle parking spaces provided for residential use must be covered, secured, and located separately from bicycle parking spaces provided for nonresidential uses.	
<b>LANDSCAPING AND OPEN SPACE</b>					
<b>Landscaping</b>	N/A	Nonresidential: 6% Minimum Residential: 20% minimum.	6% minimum	6% minimum	
<b>Usable Public Open Space – Nonresidential Uses or Nonresidential Component of Mixed-Use</b>	N/A	5% of total parcel area, minimum			Section 3.4.11 of this Specific Plan
<b>Required Landscaping of Public Open Space for Nonresidential Uses or Nonresidential Component of Mixed-Use</b>	N/A	40% of usable public open space, minimum	40% of usable public open space, minimum	50% of usable public open space, minimum	Section 3.4.11 of this Specific Plan (Additional landscaping may be required outside open space areas)
<b>Usable Private Common Open Space – Residential Uses and Residential Component of Mixed-Use</b>	N/A	150 s.f./dwelling unit minimum	N/A	150 s.f./dwelling unit minimum	Section 3.4.11 of this Specific Plan
<b>Private Usable Open Space</b>		Private open space must be open to air, not fully enclosed with walls. Private open space cannot be covered by a roof by more than 50% of the area; however, balconies can have up to 100% ceiling coverage. Private open space includes balconies, patios, or yards.			
	N/A	75 s.f./dwelling unit minimum OR 1. 60 s.f./dwelling unit minimum;	N/A	75 s.f./unit minimum OR 1. 60 s.f. /dwelling unit minimum;	Section 3.4.11 of this Specific Plan

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Specific Plan Standards	GSP-OS/P	GSP-R/C	GSP	GSP-MU	Comments
		2. Private usable open space square footage per unit and usable private common open space square footage per unit shall total at least 350 s.f./unit; and 3. Approval of the Community Development Director.  Sidewalks, walkways, equipment areas associated with usable private open space are not eligible for inclusion in the calculation.		2. Private usable open space square footage per unit and usable private common open space square footage per unit shall total at least 350 s.f./ unit; and 3. Approval of the Community Development Director.  Sidewalks, walkways, equipment areas associated with usable private open space are not eligible for inclusion in the calculation.	
<b>Private Open Space Ground Floor Dimension</b>	N/A	8' in any direction minimum	N/A	8' in any direction minimum	Section 3.4.11 of this Specific Plan
<b>Private Open Space Balcony Dimension</b>	N/A	5' in any direction minimum	N/A	5' in any direction minimum	Section 3.4.11 of this Specific Plan

Figure 3.4 Height Limits



Rosemead Garvey Avenue Specific Plan  
**Height Limits**

**Legend**

- City Boundary
- Specific Plan Area
- 75' Height Limit
- 35' Height Limit
- Variable Height Limit Setback Area

0 400 800 1,200 feet

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

## 3.4.3.1. DENSITY BONUS FOR AFFORDABLE HOUSING, SENIOR HOUSING

A residential or mixed-use development that includes 5 or more dwelling units and meets one or more of the following criteria is entitled to a density bonus and one or more incentives under State Government Code Section 65915 (also known as SB1818):

- a. Ten percent of the total units of a housing development for lower income households, as defined in Section 50079.5 of the State Health and Safety Code.
- b. Five percent of the total units of a housing development for very low income households, as defined in Section 50105 of the State Health and Safety Code.
- c. A senior citizen housing development, as defined in Sections 51.3 and 51.12 of the State Civil Code, or Mobile Home Park that limits residency based on age requirements for housing for older

persons pursuant to Section 798.76 or 799.5 of the Civil Code.

- d. Ten percent of the total dwelling units in a common interest development as defined in Section 1351 of the State Civil Code for persons and families of moderate income, as defined in Section 50093 of the State Health and Safety Code, provided that all units in the development are offered to the public for purchase.

Density bonuses for residential developments shall also apply for any residential or mixed-use buildings in the Garvey Avenue Specific Plan area, under RMC Section 17.84.

If a density bonus under State Bill 1818 is granted, density or intensity bonus associated with the provision of Community Benefits will not be granted.

Concessions considered by the City for projects complying with SB 1818 shall not include an increase in height beyond the 75' limitation (as measured from the finished grade). An increase in height above the 75' limitation would create significant adverse impacts on the Garvey

Avenue Specific Plan area and surrounding areas for the following reasons:

- 1) The Garvey Avenue Specific Plan height limit of 75' has been carefully determined to achieve the goals of the Plan while preserving the line of sight, view corridors, and open-air environment for both the Plan area and surrounding areas. Disrupting these factors would become a significant barrier to the quality of life.
- 2) The 75' height limitation has been set to preserve access to direct wind that is currently afforded to the surrounding homes and businesses. Heights more than 75' could curtail or eliminate this access, particularly the access of properties to the north to onshore/cooling winds coming from the south.
- 3) The 75' height limitation and associated development standards are crafted to allow buildings in the Garvey Avenue Specific Plan area and adjacent to the area to maintain access to sunlight. An increase in height for new buildings above the

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

75' threshold would reduce the access to sunlight.

The 75' height limitation includes the consideration for design standards. This desired goal for the Garvey Avenue Specific Plan considers scale, massing, character, and elevations that contribute to the overall quality and uniformity of design elements. Height increase above the 75' mark would detract from this.

### 3.4.3.2. GARVEY AVENUE COMMUNITY BENEFIT PROGRAM

The Garvey Avenue Community Benefit program allows for substantial redevelopment or new development projects to have an increased residential density and/or nonresidential intensity with the provision of specific community benefits. The Garvey Avenue Community Benefit Program is applicable to all parcels within the Garvey Avenue Specific Plan corridor. However, if a density bonus is granted in accordance with Senate Bill 1818, a density or intensity bonus associated with the provision of Community Benefits will not be granted.

The Garvey Avenue Community Benefit Program is based on a point system. Each community benefit type is assigned a number of Community Benefit points, as described in Table 3.5. A project may earn points from a single or multiple categories, depending on the project applicant's preference. The number of Community Benefit points earned is then translated into the increased density or FAR. The increase varies by zone and land use type as shown in Table 3.6. Table 3.6 indicates the maximum density or FAR permitted.

For example, if a retail development proposed in the GSP-R/C zone proposes to be LEED Gold certified or CALGreen Tier 1 and provides an economic feasibility study that supports the retail use, the proposed development would earn 30 bonus points. The 30 bonus points allows the proposed development to have a maximum 0.85 FAR, an increase of 0.1 above the maximum FAR without the Community Benefit Incentive.

The types of community amenities or benefits eligible to receive the Community Benefit Incentive, as shown in Table 3.5, are: public open space above the required, public park, ADA park amenities, public

parking above the required, lot consolidation, siting taller buildings within in the center of the proposed development, project sustainable design, project alternative energy production, "family friendly" development, and provision of an economic feasibility study, market study, or proforma analysis for uses other than hotel, mixed-use, and sites larger than 7 acres.

### 3.4.4. BUILDING PLACEMENT

Buildings shall be placed on the street frontage property line or front setback line as follows:

GSP-R/C or GSP zone: the building shall be placed on the property line (nonresidential use) or setback (residential use) for 60% of the lot width (Figure 3.5).

GSP-MU zone:

Less than 1.00-acre site - The building shall be placed on the property line (nonresidential use) or setback (residential use) for 60% of the lot width;

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

**Table 3.5 Community Benefit Amenities and Associated Points**

Type of Benefit Provided for the Community Benefit Incentive	Maximum Points	Basis for Calculating Points
Public Open Spaces/Public Parks Exceeding Open Space and Park Standards as set forth in this Specific Plan or other City of Rosemead plan or document.	74	<p>Public open spaces are defined as an open space area that is intended for use by all members of the community throughout the day. Public spaces may occur in the form of plazas, courtyards, parks, parklets, and greenways. If a courtyard or outdoor space is intended for a specific private use (such as a singular restaurant), then the space is not a “public open space” but rather a “private outdoor dining area”. In this case, the outdoor space would not be eligible for community benefit. Walkways or sidewalks used for pedestrian travel are not eligible for community benefit.</p> <p>If public spaces are privately-owned, these open spaces must be maintained by the property owner(s) and must remain open to the public in accordance with the Rosemead Municipal Code. Public spaces may also be deeded to the City to become public plazas or parks and under the management of Rosemead’s Parks and Recreation Department, with the City of Rosemead’s approval and acceptance.</p> <p>30 Pts: For 10% or more of the site area provided the open space and/or park is landscaped with drought tolerant turf and shrubs, provides shade trees and shade structures, shaded seating areas, bicycle racks, and trash receptacles.</p> <p style="text-align: center;">OR</p> <p>50 Pts: For 15% or more of the site area provided the open space and/or park is landscaped and amenities provided as described above, and additional amenities are provided such as: stage, band shell, play structures, public restrooms.</p> <p>24 Pts: For each Accessible or Inclusive Playground Equipment designed and constructed in accordance with ADA requirements, 4 points will be awarded. Equipment qualifying for this bonus includes: ramps with a 1:12 slope and transition decks, safety surfaces, ADA swings, ADA slide and play structure, ADA drinking fountains, ADA picnic tables, ADA benches, up to a maximum of 24 points.</p>
Alhambra Wash Public Park or Open Space	45	25 Pts: For the capping of the Alhambra Wash and development of usable, landscaped park space

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Type of Benefit Provided for the Community Benefit Incentive	Maximum Points	Basis for Calculating Points
		25 Pts: For the development, landscaping, and maintenance of a public park.  Density or intensity bonus must be applied to a single building or structure located on the project parcel adjacent to the Alhambra Wash.
Public Parking	50	2 Pts: For every 1 standard sized parking space marked for public use and permanently available for public use, provided the project meets the minimum number of required public and private spaces, per this Specific Plan or the City of Rosemead.
Lot Consolidation	35	For every one or more parcels or lots that are consolidated to form a single parcel or lot and the lot consolidation is recorded with the City of Rosemead, 35 points will be awarded for each final parcel or lot.
Taller Buildings Center of Development	40	If buildings fronting Garvey Avenue, fronting San Gabriel Boulevard, or adjacent to existing residential uses are no more than 48' tall.
Sustainable Design	70	40 Pts: If 50% or more of total building roof is an accessible, operational eco roof.  30Pts: LEEDTM Platinum, CALGreen Tier 2, or equivalent (third-party certification required)  20 Pts: LEEDTM Gold, CALGREEN Tier 1, or equivalent (third-party certification required)  The increased density or intensity will be granted to the qualifying building not the entire development or site area.  The project will be conditioned to ensure compliance and construction in accordance with LEED Platinum, LEED Gold, CALGreen Tier 2, or CALGreen Tier 1.
Alternative Energy	30	If 25% of total building energy load is provided by solar panels or other on-site renewable sources, provided the other on-site sources are approved by the City of Rosemead, local energy regulators, and local air quality district, as appropriate.
Family Friendly Development	50	30 Pts: Projects providing more than 10% of housing units as three bedroom or larger units.  20 Pts: 1 point for each 15 s.f./unit of common area open space above the required minimum as stated in this Specific Plan, provided the common area open space contains at least two of the following: tot lot play equipment (swings, slide, climbing structure), community garden, or library, up to a maximum of 20 points.

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

Type of Benefit Provided for the Community Benefit Incentive	Maximum Points	Basis for Calculating Points
Economic Feasibility Study Market Study Proforma Analysis	10	Provided the economic feasibility study, market study, or proforma analysis is prepared by a reputable professional economic or market analysis firm, the City of Rosemead affirms economic feasibility study author’s credentials and economic feasibility study’s findings, and the economic feasibility study supports the proposed land use. The economic feasibility study Community Benefit is applicable to uses other than hotel, mixed-use, and sites greater than 7 acres.
Nonresidential Component of Mixed-Use Development Sites	20	In order to provide for significant opportunities for national and regional retail tenants, a bonus shall be granted if the nonresidential component of a mixed-use site provides for tenant space with an average size of 2,000 s.f. or more (minimum size of 800 s.f. for each tenant space), then the project will receive a 5% increase in residential to make the split 70% residential to 30% commercial.

**Table 3.6 Community Benefit Incentive Maximum FAR or Density**

Maximum FAR or Density Permitted With the Provision of Community Benefits						
Community Benefit Points Earned	GSP-R/C Zone		GSP Zone	GSP-MU Zone		
				Commercial Land Use	Mixed-Use Land Use	
	FAR	Density	FAR	FAR	FAR	Density
0	0.75	7	0.75	0.75	1.6	25
1-20	0.8	11	0.8	0.8	1.78	32
21-40	0.85	14	0.85	0.85	1.96	39
41-60	0.9	18	0.9	0.9	2.14	46
61-80	0.95	21	0.95	0.95	2.32	53
81-100	1.0	25	1.0	1.0	2.5	60
101-115	N/A	N/A	N/A	1.0	2.68	67
116-130	N/A	N/A	N/A	1.0	2.86	74
130 and above	N/A	N/A	N/A	1.0	3.0	80

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

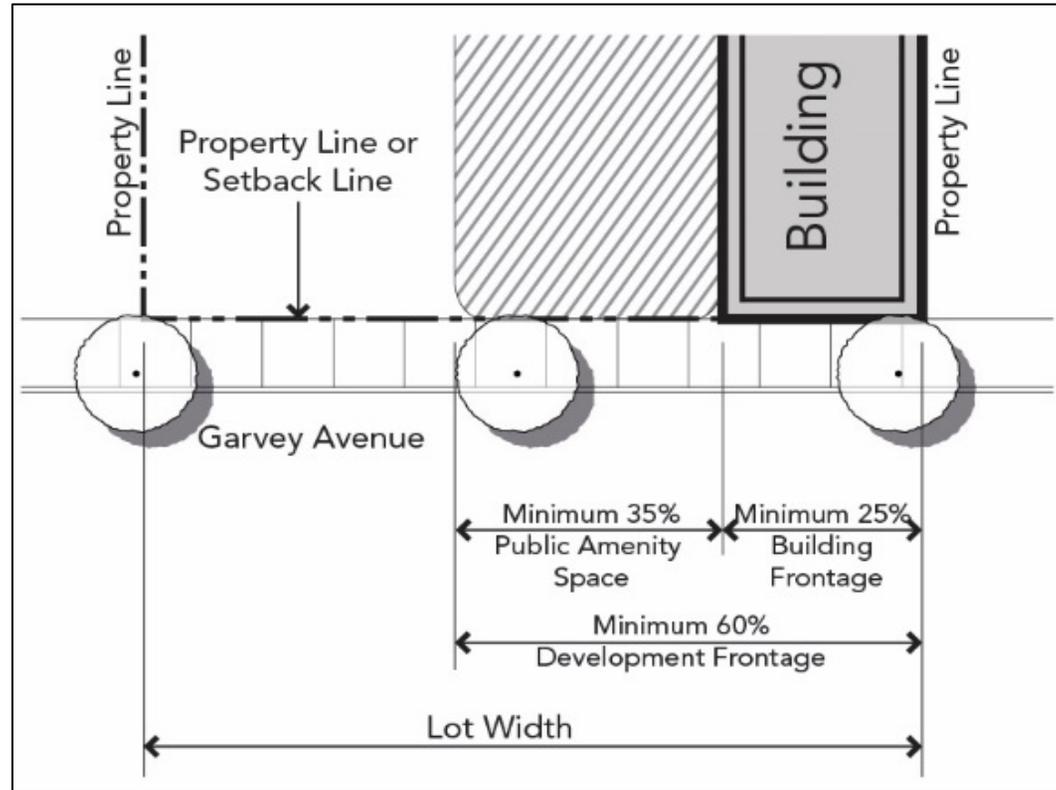
1.00-acre site to 6.99-acre site – Sites must use the building or other vertical features to create a building placement equal to 60% of the lot width.

Other vertical features include:

- highly landscaped vertical decorative walls screening parking (maximum 3’ height);
- highly landscaped and amenitized open spaces or public plazas that incorporate a decorative wall or other vertical feature (maximum 3’ height); or
- outdoor dining.

When using other vertical features, building placement on the property line (nonresidential) or setback line (residential) must equal a minimum of 25% of the lot frontage; landscaped vertical decorative screening parking wall, landscaped and amenitized open spaces incorporating a vertical wall/feature, or outdoor dining must equal no more than 35% of the lot frontage. The vertical feature wall’s design and placement must

**Figure 3.5 Building Placement**

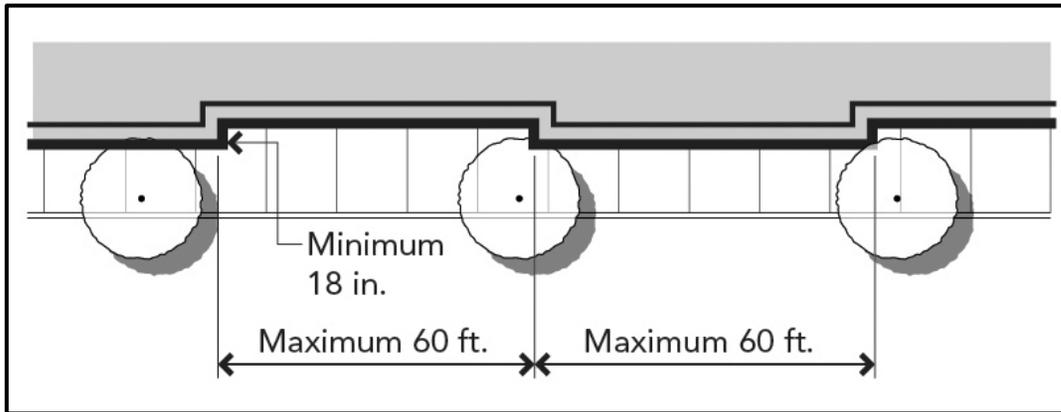


be approved by the Community Development Director;

7.00-acre site or greater - The building shall be placed on the property line (nonresidential use) or setback (residential use) for 60% of the lot width.

When the building frontage is on Garvey Avenue, a second floor’s building overhang (drip line) may be considered the building frontage if the outdoor ground floor contains a restaurant dining space or pedestrian plaza. The outdoor dining space or the outdoor pedestrian plaza must

**Figure 3.6 Façade Plane Modulation**



be designed for such use and include both decorative hardscape and landscape.

The Review Authority may grant exceptions for:

- a. A narrow lot under 50' in which a 24' driveway is necessary because no side street, alley, or easement can provide access to required parking on the rear of the lot or site; and
- b. The initial phases of a multi-phased building project that will occupy the entire frontage upon completion.

**3.4.5. BUILDING DESIGN**

As previously stated, the design standards and guidelines of this Garvey Avenue Specific Plan are largely consistent with those in the Garvey Avenue Master Plan and Rosemead's Mixed-Use Guidelines. The two previous plans have already extensively addressed guidelines for building design that should be followed in the Garvey Avenue Specific Plan area. However, the Garvey Avenue Specific Plan underscores the importance of high-quality building design in all development within the Garvey Avenue Specific Plan area. Building modulation is required. Porches, patios, stoops, and terraces at the entrances of ground-related units,

sidewalk arcades fronting commercial uses, and open-to-the-air roof terraces and shallow step-backs at the top floors of structures are encouraged to create transitions from public to private spaces, emphasize sheltering at entrances, encourage indoor and outdoor use of ground-floor space, relate new buildings to existing buildings, and to create human-scale massing accents within the context of overall building bulk.

All buildings constructed in the Garvey Avenue Specific Plan area shall be designed to maximize the privacy of the adjacent homes, backyards, and residential neighborhoods.

Façade plane modulation is required. The wall plane of street-facing façades shall be modulated a minimum of 18" perpendicular to the plane at least once every 60', as measured along the property line (Figure 3.6).

New buildings and additions to existing buildings shall incorporate at least one or more of the following types of modulation:

- Façade Modulation Elements. Façades should be modulated with

# LAND USE, ZONING, AND DEVELOPMENT STANDARDS

elements including but not limited to vertical and horizontal breaks in the building façade plane, setbacks at upper levels, changes in material or color, use of ornament, changes in height, and incorporation of other design elements that create differentiation in the architecture to create visual and architectural interest, see Figure 3.6.

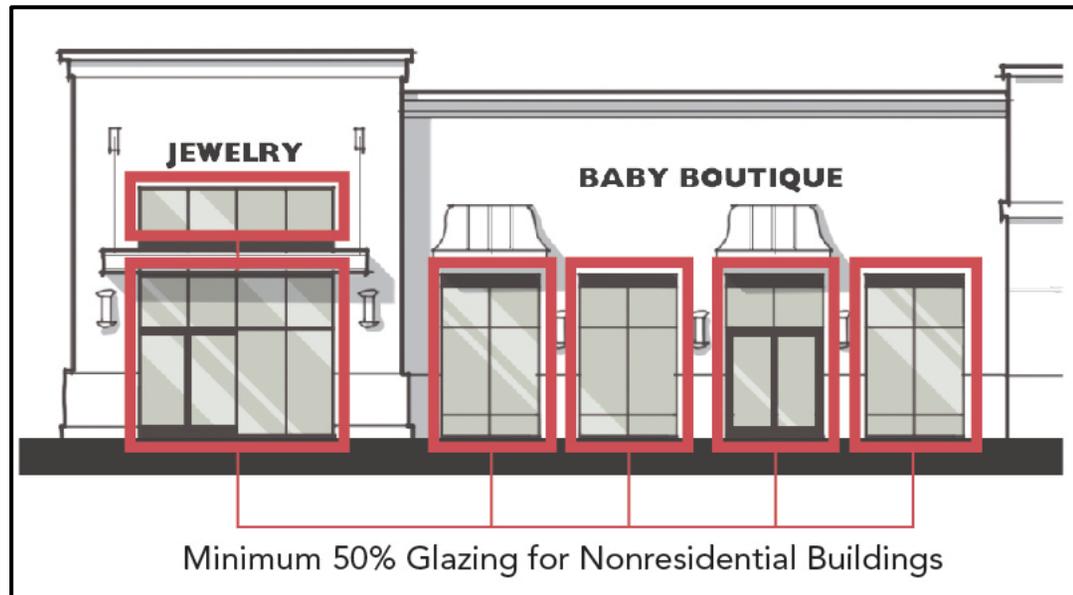
- Building modulation. New buildings and additions should utilize modulation of mass and bulk to reference adjacent building heights and existing massing patterns seen along Garvey Avenue to create transitions and relationships between new and old and to relate the scale and massing of new structures to the scale and massing of the surrounding neighborhoods.
- Façade depth modulation. Windows within building facades should emphasize individual windows that are setback into walls to create distinct patterns of shade and shadow.
- Façade material modulation. Use of high-quality materials like smooth

finish stucco, brick, wood, and stone are encouraged.

Street-facing facades of all buildings shall incorporate glazing (windows) and openings providing light to adjacent spaces, rooms, and uses (Figure 3.7). As outlined in Table 3.4, windows and openings facing streets shall constitute a minimum of the following percentage of street-facing building faces:

- 50% of the wall for nonresidential ground floor uses;

**Figure 3.7 Minimum Building Glazing**



- 30% of the wall for nonresidential upper floor uses;
- 40% of the wall for residential ground floor uses; and
- 30% of the wall for residential upper floor uses.

### 3.4.6. ROOF AND SKYLINE FORMS

The design of varied roof and skyline forms including pitched roofs, variation in roof heights on a single building, high-quality roof materials (such as tile and metal), corner towers, and mixing of pitched and flat roofs is encouraged.

#### **Design of roof and skyline forms**

Penthouses, parapets, stair and elevator enclosures, and air conditioning units and mechanical equipment shall be fully integrated into the overall architectural design and expression of the building or addition through the use of equivalent materials and colors that match the overall design. All rooftop mechanical components shall be fully screened from the view of public right-of-ways or residentially zoned properties.

### 3.4.7. BUILDING ENTRANCES

For parcels fronting Garvey Avenue, the primary entrance to a ground floor use shall face Garvey Avenue. Residential uses should have their own on-street entrance, while nonresidential uses abutting Garvey Avenue shall provide at least one street-facing, pedestrian-priority entrance that shall serve as the primary entrance to the business.

#### **Pedestrian-priority primary entrances on Garvey Avenue**

Interior space shall be arranged to orient toward the Garvey Avenue entrance as the primary entrance. Signage shall be used to direct persons toward the primary entrance. Street-facing nonresidential entrances shall be unlocked and accessible to the public during business hours.

#### **Residential ground-floor, on-street entrances**

Residents of a mixed-use development shall have a separate and secure on-street pedestrian access to the residential units. To enhance the sense of connection between ground floor residential units and adjacent right-of-ways, entries to these units should incorporate stoops, front yards separated from sidewalks by low

walls and gates, entry alcoves, awnings, canopies, architectural accents, surrounds, and details.

#### **Entrance Frequency**

On street frontages, ground-related entrances shall occur at least once every 150', as measured along the front property line. Ground-related entrances include entrances to ground-floor uses, residential units, clusters of residential units, lobbies, or private courtyards.

#### **Paseos and pedestrian walkways**

Pedestrian walkways or sidewalks shall connect all primary building entrances to one another. Pedestrian walkways shall also connect all onsite common areas, parking areas, storage areas, public open spaces, and recreational facilities. Public paseos are strongly encouraged to enhance pedestrian mobility throughout the Garvey Avenue Specific Plan area for all residents and visitors.

#### **Elevated walkways, skyways, and pedestrian bridges are limited**

One of the Garvey Avenue Specific Plan purposes is to encourage Garvey Avenue's pedestrian orientation, walkability, and street life. Elevated walkways, skywalks, and bridges

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decrease the likelihood of pedestrians walking at street level. As such, elevated walkways, skyways, and bridges between buildings are limited. Elevated walkways, skyways, and bridges between buildings are allowed under the following conditions:

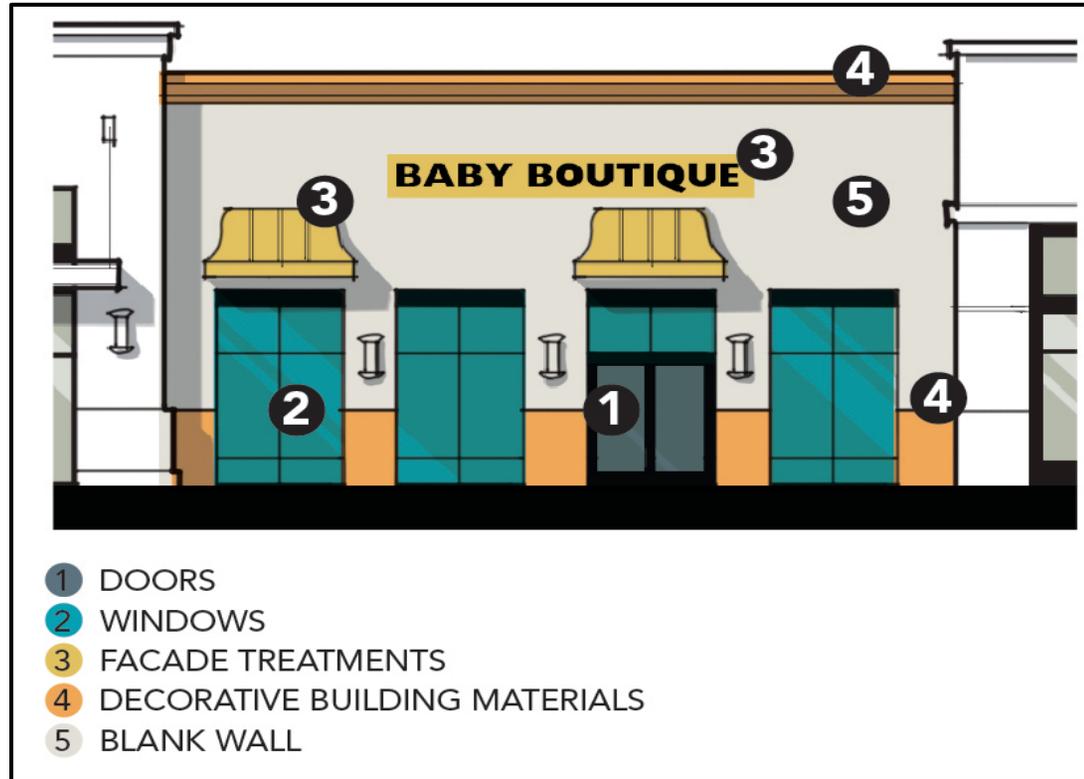
- Elevated walkways, skyways, or bridges shall connect buildings located internally to a site. Elevated walkways, skyways, or bridges shall not connect buildings located on Garvey Avenue, San Gabriel Boulevard, nor be adjacent to an existing residential use.
- An elevated walkway, skyway, or bridge is only allowed to facilitate resident access to an adjacent buildings' recreational amenity.
- Each building shall be connected to no more than one other building.
- A maximum of one elevated walkway, skyway, or bridge is permitted per building. For example, if an elevated walkway connects two buildings on their third floors, no other elevated walkway is permitted

to connect any other on any other floor.

- Elevated walkways, skyways, or bridges shall not cross over a street, public park, or public open space.

Deviations from the above standards may be granted by the Community Development Director if it is determined that such deviation will not decrease the likelihood of pedestrians walking at the street level, and any excess elevated walkways, skyways, and pedestrian bridges will not be visible from any public right-of-way.

**Figure 3.8 Example Nonresidential Building Façade**



## 3.4.8. GROUND FLOOR DESIGN

Ground floor design shall be high-quality, pedestrian-oriented, and sensitive to the use. Residential units' ground floor design should be significantly different than the design for nonresidential. Storefront configurations and details providing a sense of human scale, variety, and interest within the overall context of buildings bays and groupings of bays are encouraged. Such details include but are not limited to recessed entries at storefronts, recessed storefronts, display windows, projecting bays, integral awning, utilization of true dividing mullions, transoms over entries, and integral signs and sign boards, see Figure 3.8.

### Ground floor residential units allowed

In mixed-use settings, ground floor residential dwelling units are allowed on any lot and on any street frontage, provided storefronts and usable commercial space are located along a minimum of 50% of the length of the building frontage adjoining Garvey Avenue. Storefronts and usable nonresidential spaces shall comply with all standards and guidelines in

Rosemead's Mixed-Use Design Guidelines and of Section 5.5.1.

*First Floor Elevation: Nonresidential uses*

The first habitable floor shall be located no more than 2' above or below the finished grade at any point along a street property line.

*First Floor Elevation: Residential uses*

The first habitable floor of a residential-only building shall be located at least 2' above finished grade and no more than 3' along a street property line.

### Minimum ground floor height

The minimum height of nonresidential ground floor spaces shall be 14'. The minimum height of residential ground floor spaces shall be 10'. This height shall be measured from the floor of the first story to the floor of the second story. If there is no second story, the height shall be measured to the top of roof, see Figure 3.9.

### Storefronts and nonresidential uses required at ground floors

Storefronts and usable nonresidential space shall be located along the required

**Figure 3.9 Building Height**



ground floor street frontages of buildings per Table 3.4 of this Plan and have a minimum usable depth of 40' along 60% of the length of the building frontage and in no case be less than 20' in depth. At a corner lot where storefronts and nonresidential uses are required, storefronts and nonresidential spaces shall turn and wrap around the corner for a minimum length of 20', see Figure 3.10.

### Glazing at ground floors

Use of mirrored and reflective glazing materials and glass is prohibited. At the ground floor of nonresidential buildings, use of clear glass is required. After installation, clear glass windows at the

ground floor of nonresidential uses shall not later be treated so as to become opaque or to be blocked so as to prevent visibility of the ground floor interior from the sidewalk. However, window signs should not cover more than 15% of the area of each window, in accordance with the Garvey Master Plan.

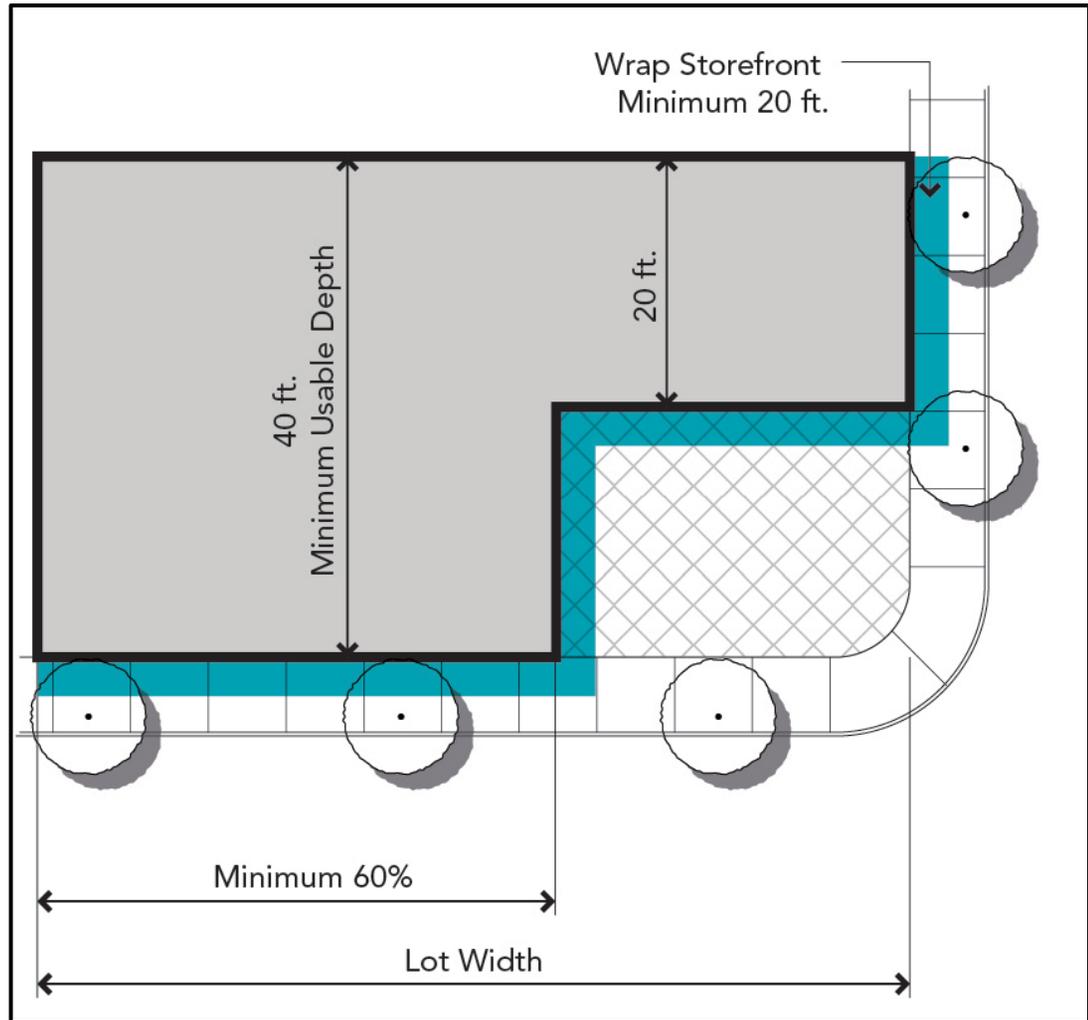
**Awnings and trellises**

Awnings and trellises are permitted above ground floor and second story doors and windows. Permanent structural trellises are permitted above doors and windows on ground floor and higher stories.

**Security elements and decorative grillwork at ground floors**

Upward rolling, side folding, or moveable security grills and elements shall not be installed on the exterior side of ground floor and street-facing storefronts, windows, openings, entries, and facades. On the interior side of ground floor and street-facing storefronts, windows, openings, entries and facades, upward rolling, side folding, or moveable security grills and elements are allowed and should be designed to be integral to the architecture of the building and opening.

**Figure 3.10 Minimum Usable Depth**





Decorative grillwork examples.

Such devices should utilize dedicated interior side pockets and ceiling cavities such that the grill and all mechanisms associated with the security element are not visible from the adjacent public right-of-way and sidewalk; the grill work, regardless of installation method, shall be at least 80% open to perpendicular view. Fixed decorative grillwork and railings are allowed at the ground floor and shall be at least 80% open to perpendicular view.

### 3.4.9. PARKING STANDARDS AND DRIVEWAYS

Required parking shall be determined by the standards outlined in Table 3.4 of this Garvey Avenue Specific Plan. Parking standards and driveways shall comply with the following requirements:

#### Parking location

Surface parking along Garvey Avenue is not allowed between the building and Garvey Avenue or between the decorative wall/landscaping or plaza and Garvey Avenue. Surface parking in the rear of a lot shall be permitted. Notwithstanding the requirements of Table 3.4, partially subterranean and fully subterranean

parking may extend to street-fronting property lines. For corner lots, surface parking lots shall be accessed from a side street or alley.

#### Residential and nonresidential separation

Residential parking may share an entrance with nonresidential parking, but shall be separate from the nonresidential parking area, with access restrictions where necessary.

#### Driveway or curb cut frequency

For parcels of less than 300' in length, only one vehicular access point may be permitted. For all other lot frontages, a maximum of one vehicular access point for each 300' of street frontage is permitted, see Figure 3.11. Any variations to the curb cut frequency requirements must be approved by the Community Development Director and City Engineer, or City Manager.

#### Landscape buffers

On-site parking that is visible from streets and sidewalks or located along rear, side, or interior lot lines shall be buffered from the street, sidewalk, or lot line by a minimum 5' in-depth landscape buffer. Further, parking that is visible from

Garvey Avenue shall be buffered from the street, sidewalk or lot line by a minimum 5' in-depth landscape buffer that includes a decorative wall no higher than 3' to screen the vehicles.

### Tuck-under parking

Openings to tuck-under parking spaces shall not be visible from the street or from an adjacent property.

### Tandem parking

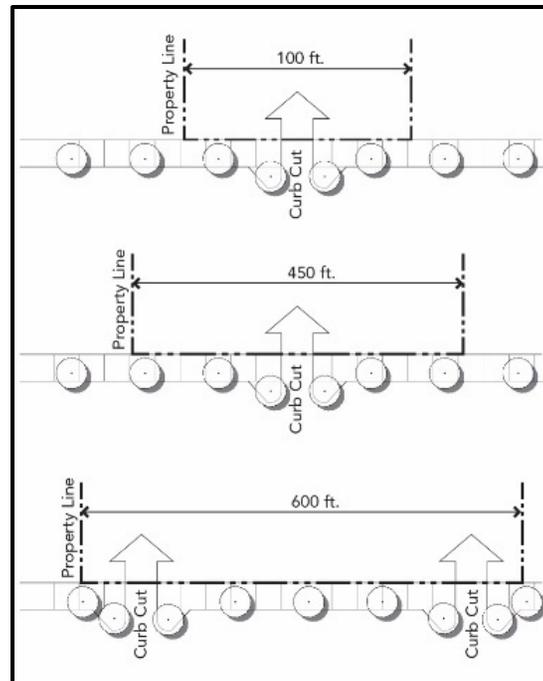
Tandem parking within allowed parking areas may be allowed with an AUP for multi-family projects and the residential component of mixed-use projects. Tandem parking is permitted for nonresidential uses with a CUP. When a nonresidential use's tandem parking is permitted, free valet service shall be conditioned, if needed.

### 3.4.10. LOADING SPACE

On the same premise with every building, structure, or part thereof erected or occupied for goods display, wholesale or retail, hotel, restaurant, or other similar use involving the receipt or distribution of materials or merchandise via trucks or vans, a minimum of one off-street loading space for each 6,000 s.f. of building floor

area dedicated to the retail, commercial, hotel, restaurant use is required for the standing, loading and unloading services in order to avoid interference with the public use of adjacent streets or alleys. Required loading space shall not be included within the required parking space adjacent to a building or structure.

**Figure 3.11 Curb Cuts**



### 3.4.11. OPEN SPACE AND RECREATION

Each type of open space has a different purpose and user; the requirements for one type of open space cannot be satisfied by another.

#### Usable Public Open Space

- a. All open space shall be public unless parcel location does not allow public access.
- b. Open space amenities for public use such as trees, seating, and recreational amenities should be provided in open space. High-quality porous pavers, porous concrete or other porous paving materials shall be used for all plazas and hardscape. Shade trees should be provided in all open space. Parks and greenways should be designed with high-quality benches, lighting, paving, and landscaping.
- c. Community Benefit Incentives are available for providing more usable public open space than is required, see 3.4.3.2.

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- d. Usable public open space does not include libraries, gymnasiums, nor recreation rooms.
- e. All required usable public open space shall be developed and professionally maintained in accordance with approved landscape and irrigation. All landscaping should use high-quality materials.

## Usable Common Open Space

- a. All usable common open space shall be conveniently located and readily accessible from all residential units in a residential or mixed-use development.
- b. Each usable common open space shall have no side with a dimension of less than 10' and may incorporate any areas of the site except where it is within 5' of public rights-of-way, private streets and alleys, and shall not include or incorporate any driveways or parking areas, trash pickup or storage areas, pool equipment, or utility areas.
- c. Usable common open space may be provided on a rooftop where mechanical equipment is located, if

the open space is adequately mitigated for noise, odor, vibration, or other impact and is approved by the Community Development Director and Building Official.

- d. There shall be a minimum distance of 15' measured horizontally between any swimming pool and the nearest point of any balcony, porch, second story patio, sun deck, or other architectural feature of a building or structure with windows, doors, or other openings of sufficient size to permit the passage of persons.
- e. Courtyards internal to a project, or enclosed on at least two sides, shall have a minimum width of 40', and shall be landscaped with a ratio of hardscape to planting not exceeding a ratio of one square foot of landscape to one square foot of hardscape. Pools and spas shall be excluded from this ratio.
- f. All required usable common open space shall be developed and professionally maintained in accordance with approved landscape and irrigation. All landscaping should use high-quality materials.

## Open Space

Vibrant, thriving communities or neighborhoods provide a variety of open spaces for different users.

**Usable Public Open Space** provides placemaking, community gathering spaces and provides a welcoming pedestrian orientation for the entire community. Usable Common Public Open Space can occur in the form of plazas, recreation areas, parks, etc.

**Usable Common Open Space** provides gathering space or recreation facilities/amenities to the residents or users of a particular neighborhood, or condominium or townhouse development. Usable Common Public Open Space can occur in the form of a pool area, tennis courts, private park or tot lot, etc.

**Usable Private Open Space** provides an individual, family, or user their own personal open space. Private Open Space occurs as a home's backyard, balcony, or patio.

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- g. Usable common open space may include libraries, gymnasiums, or recreation rooms.

## Usable Private Open Space

- a. No portion of any private patio or balcony shall be used for the permanent storage of rubbish, junk, clotheslines, or garbage receptacles. "Permanent storage," as used in this subsection, means the presence for a period of 48 or more consecutive hours on a patio or balcony.
- b. Usable private open space must be open air, not fully enclosed with walls. Usable private open space may be enclosed with a 3' high wrought iron rail, partial wall, or other complementary material as permitted by the Community Development Director. Usable private open spaces cannot be covered by a roof by more than 50% of the area; however, balconies can have up to 100% ceiling coverage.

## 3.4.12. SIGNAGE STANDARDS

All signage shall comply with guidelines outlined in the Garvey Avenue Master Plan, with the exception of mixed-use

projects, which shall follow the sign standards outlined in Title 17, Article 2, in Section 17.28.030. Bilingual signage is allowed, and encouraged, in areas where shoppers are most likely to benefit from it. When storefronts use bilingual signage, nonresidential signage shall identify the type of business (e.g. "restaurant", "market") in the English language and should be clearly located in the center of the storefront signage.

## 3.4.13. CONDOMINIUM SUBDIVISION STANDARDS

Nonresidential and residential condominiums are permitted in the GSP-R/C and GSP-MU zones. All condominium subdivisions within the Garvey Avenue Specific Plan area, whatever the parcel size, shall be processed and developed in accordance with the provisions of the development standards and requirements set forth in this Garvey Avenue Specific Plan. Existing provisions in Rosemead's Zoning Code may be applicable as determined by the Community Development Director.

All condominium applications shall

include:

- 1) Proposed Site Plan
- 2) Preliminary Grading Plan
- 3) Preliminary Landscaping Plan
- 4) Preliminary Lighting Plan
- 5) Preliminary Master Sign Plan
- 6) Proposed Condominium Documents
- 7) Delineation of Shared Spaces and Access
- 8) Other Information that the Community Development Director determines may be necessary to evaluate the proposed project to ensure consistency with the Garvey Avenue Specific Plan, Rosemead's General Plan, the Zoning Code, and other applicable City regulations and requirements.

Earned community benefit incentives may be applied over entire project area, even if the property is subdivided for residential condominiums or other financing purpose, with the approval of the Community Development Director.



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# Chapter 4

## OPPORTUNITY PROJECTS AND SITES

### IN THIS CHAPTER

- 4.1 Garvey Avenue LA Auto Auction and Landwin Property Sites Mixed-Use Destination**
- 4.2 Garvey Avenue “West Gateway” Specialty Retail Destination**
- 4.3 Garvey Avenue “Prototypical Development” Opportunity Sites**

The ideas and inspirations on the following pages represent vital, tangible opportunities to re-energize and catalyze development along Garvey Avenue. Taken together, they provide a vision for a revitalized Garvey Avenue, with successful new businesses in previously vacant storefronts, new residents in mixed-use developments with active ground floor retail on formerly empty and desolate parcels, and a vibrant new streetscape design that calms traffic and welcomes pedestrians and bicyclists. Garvey Avenue’s catalytic projects have the potential to remake the area into a vibrant place and a neighborhood, with a bright future.

In the future, a transformed LA Auto Auction site becomes a central mixed-use walkable community destination, filled with stores, outdoor dining, entertainment venues, parks, greenways, and new homes. A re-imagined Landwin Property site complements the redeveloped LA Auto Auction site with a new hotel, ground floor retail and open space along a revitalized Alhambra Wash – the first big step in achieving a major new greenway corridor. A vacant site at Prospect Avenue becomes a specialty shopping hub for the neighborhood and draw residents from other nearby neighborhoods. Up and down Garvey Avenue, vacant or underutilized sites are redeveloped to their highest and best use, with new high-quality nonresidential and mixed-use buildings that bring life, energy, and people to the street.

Figure 4.1 shows the breadth and overall vision for catalytic development projects on Garvey Avenue. These are visions — examples of what may occur on the sites, but is not required.

## 4.1. GARVEY AVENUE LA AUTO AUCTION AND LANDWIN PROPERTY SITES MIXED-USE DESTINATION

A once-in-a-lifetime development opportunity, the former LA Auto Auction and Landwin Property sites have the greatest potential to transform Garvey Avenue and be a catalyst for further development up and down the street. It is an unprecedented opportunity to guide development in ways that can make and spur dramatic and tangible improvements in the livability and attractiveness of the entire street. The LA Auto Auction site is both the largest parcel and the largest vacant parcel in the Garvey Avenue Specific Plan area, over 17 acres in size. It is located on the north side of Garvey Avenue between Strathmore Avenue and Pine Street, and centered on Kelburn Avenue. It directly borders another key opportunity project site, the Landwin Property, which extends from the eastern boundary of the LA Auto Auction site to Alhambra Wash, with a small extension through to San Gabriel Boulevard. Though the two properties are owned separately, for purposes of an overall integrated development vision for this

area, the two sites are considered together here. Figures 4.2 and 4.3 show the location and existing condition of the Former LA Auto Auction site and the Landwin Property.

### **The Former LA Auto Auction and Landwin Property Sites Today**

Due to its size and development potential, the former LA Auto Auction site currently has a special designation in Rosemead’s General Plan as a “high-intensity” commercial area. It is currently zoned C-4 Regional Commercial. The zoning effectively calls for the former LA Auto Auction site to be developed with “big box” and chain-store retail, along with a hotel. To accommodate this kind of development, much of the land would be developed with surface parking lots. The development would be essentially car-oriented with little to no pedestrian-orientation; as a result, new car traffic from around the entire region would be expected to significantly increase. The existing zoning and development designation is out of step with the needs and character of the surrounding neighborhoods, and does not represent the kind of development envisioned for Garvey Avenue in either the Garvey

Figure 4.1 Map of Opportunity Projects and Sites

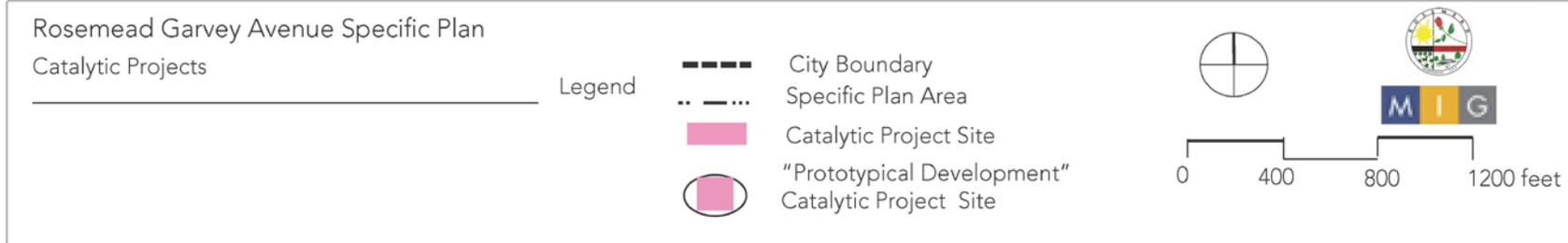
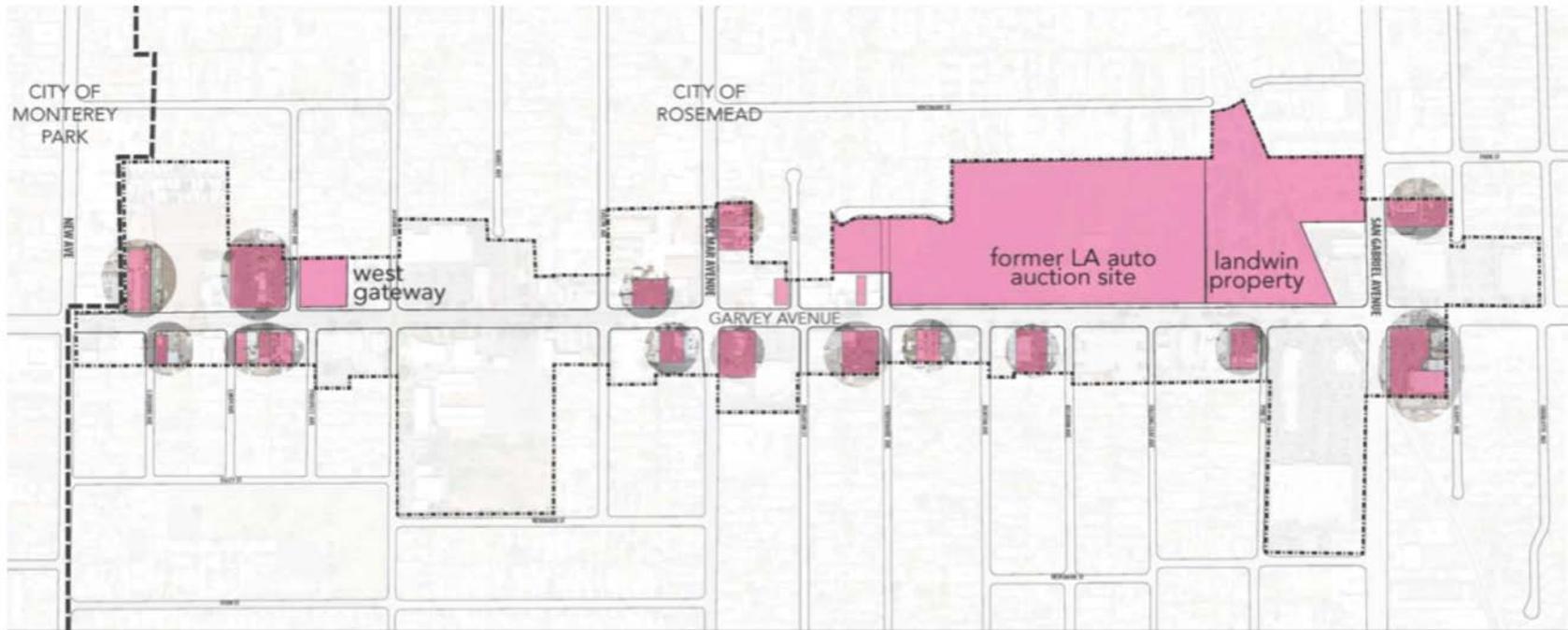


Figure 4.2 Aerial Map of LA Auto Auction and Landwin Property Sites





Avenue Master Plan or this Garvey Avenue Specific Plan. It would significantly change Garvey Avenue, but largely in negative ways. Furthermore, due to the site's location not on a freeway or near a major crossroads, the kind of large-scale, regional big-box development currently envisioned for the site is not considered economically feasible.

The Landwin Property, sited directly east of the former LA Auto Auction site (Figure 4.4), has similar transformative potential. Much of the Landwin Property sits empty and undeveloped today. The property straddles Alhambra Wash, a major green amenity that is not being utilized to its greatest potential and is fenced off from the public. The Landwin Property is a large combined parcel, though not nearly as large as the LA Auto Auction site. Its western half is currently in use as the Paradise Trailer Park. Though the trailer park may not be the site's highest and best use, it is home to a number of people.

### The Vision

The Garvey Avenue Specific Plan envisions and suggests an entirely different kind of development for the LA Auto Auction and Landwin Property sites,

a mixed-use destination that is pedestrian-oriented, neighborhood- and small business-focused, and with a network of parks, open spaces, and greenways instead of large surface parking lots. It could accommodate a significant amount of commercial and entertainment development, but in a far more livable and sustainable way that develops the land more efficiently and cost-effectively.

The LA Auto Auction and Landwin Property could create high-quality public parks, open spaces, and greenways, and build a significant amount of attractive residential development to accommodate Rosemead's growing population. Parking would be right-sized, centralized, and tucked out of sight to minimize its negative impacts as much as possible. Safe, pedestrian-friendly new neighborhood streets would reconnect through the site; the scale and character of the surrounding neighborhoods would be respected throughout the development. The entire area would be neighborhood, rather than regional-focused.

### Design Principles

The Garvey Avenue Specific Plan identifies seven strongly recommended key design principles for the LA Auto



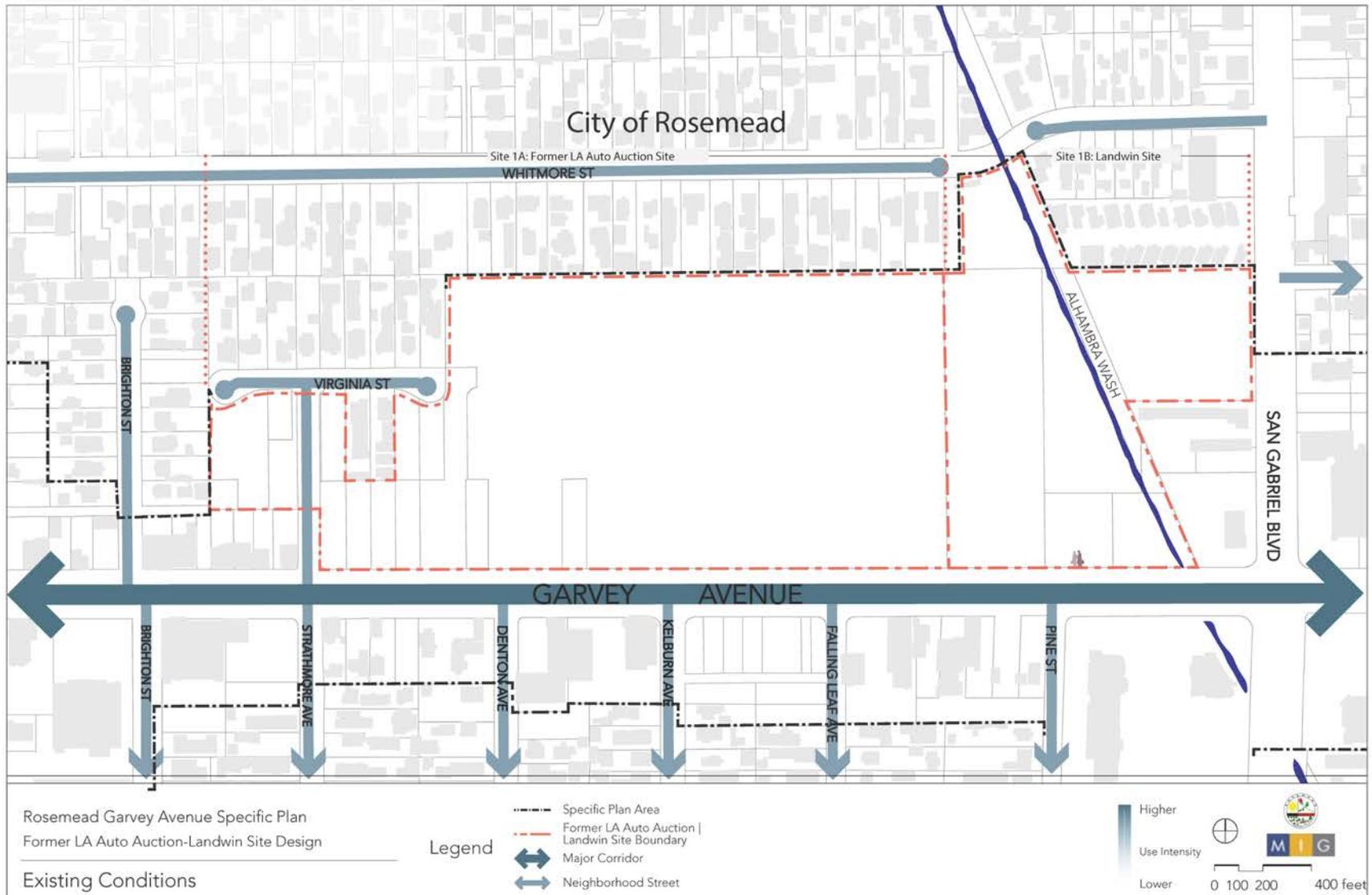
**Nonresidential building height and massing with a public plaza.**

Auction and Landwin Property sites. All achievable principles shall be applied to each individual project.

### Restore the Street Grid for Ingress and Egress

- Extend Denton, Kelburn, Falling Leaf, and Pine Streets across Garvey Avenue and into the site, connecting to Virginia Street, which would itself be extended east to San Gabriel Boulevard. Note for environmental review purposes, a minimum of three new connections to Garvey Avenue and one new connection to San Gabriel Boulevard is assumed.

Figure 4.4 LA Auto Auction and Landwin Property Site Map, 2014



- Include paseos, midblock crossings, and pedestrian passageways to further break up the street grid and ensure full pedestrian connectivity.

## Foster Walkable Neighborhoods

- Improve pedestrian safety on Garvey Avenue, including new midblock crossings and enhanced existing crossings, whenever possible.
- Narrow and improve Garvey Avenue crossings at Brighton, Strathmore, Denton, Kelburn, Falling Leaf, and Pine Streets for pedestrian and driver safety.
- Narrow and improve San Gabriel Avenue crossings into the site, at Virginia and Park Streets.
- Design blocks at a pedestrian scale, including alleys and mews, and keep new streets narrow for pedestrian safety and livability.
- Invest in a new network of greenways that connect open spaces, residential, and retail areas, and

encourage active transportation and recreation.

## Restore Alhambra Wash

- Capitalize on development on parcels bordering Alhambra Wash to begin restoring this natural feature and revitalize this open space amenity.
- Best efforts to provide public access along the Wash itself, and provide new public greenways to be built along the creek edges with new development, whenever possible.
- Highlight the Wash at Garvey Avenue with wayfinding, educational signage, and special design features.
- Study extending the greenway along the Wash throughout Rosemead, where possible, to eventually connect to Rio Hondo and Whittier Narrows Recreation Area.

## Active Garvey Avenue Street Frontage

- Redevelop sites with active, pedestrian-friendly ground

floor retail along Garvey Avenue.

- Ensure the entire street wall along Garvey Avenue is activated with retail for the length of the site.
- Enhance Garvey Avenue's streetscape with new landscaping, street trees, lighting, street furniture, bike parking, and other amenities.

## Design Sensitive Transitions

- Ensure new and significantly remodeled development follows design guidelines in this Garvey Avenue Specific Plan. Ensure all nonresidential development follows the Garvey Avenue Master Plan's Design Guidelines; and ensure new mixed-use development follows the design guidelines in Rosemead's Mixed-Use Guidelines.
- Use context-sensitive design to ensure all new development respects the scale and character of the surrounding residential neighborhoods.
- Build smaller buildings along site edges and larger buildings

internally; require building setbacks to soften the impact of higher buildings.

- Use green buffers along property lines and greenways and creek buffers along Alhambra Wash to respect neighbors and ensure public access to key natural features.
- Use green buffers and cladding on and around any stand-alone parking facilities.

## Serve and Support Surrounding Neighborhoods

- Ensure all development opens to and welcomes residents from surrounding neighborhoods, rather than being internally-focused, closed off, and unwelcoming.
- Ensure permeability by designing a network of streets, alleys, and greenways that connects to and through the site from neighborhoods.
- Include neighborhood-serving retail that nearby residents can walk and bike to and family-friendly

entertainment that appeals to Rosemead’s residents.

## Design with Tallest Buildings Internal to Site

- Build tallest buildings internally, set back from Garvey Avenue, San Gabriel Boulevard, and the residential neighborhoods to the north and west.
- Capitalize on Community Benefit Incentives to bring needed community benefits to the neighborhood like open space and parks.
- Consider wind and shadow impacts; assure minimal shadows on parks and open spaces.

A diagram series in Figure 4.5 illustrates these design principles.

## Recommended Design Concept

For purposes of development integration, the Garvey Avenue Specific Plan design concept considers the former LA Auto Auction site and the Landwin Property

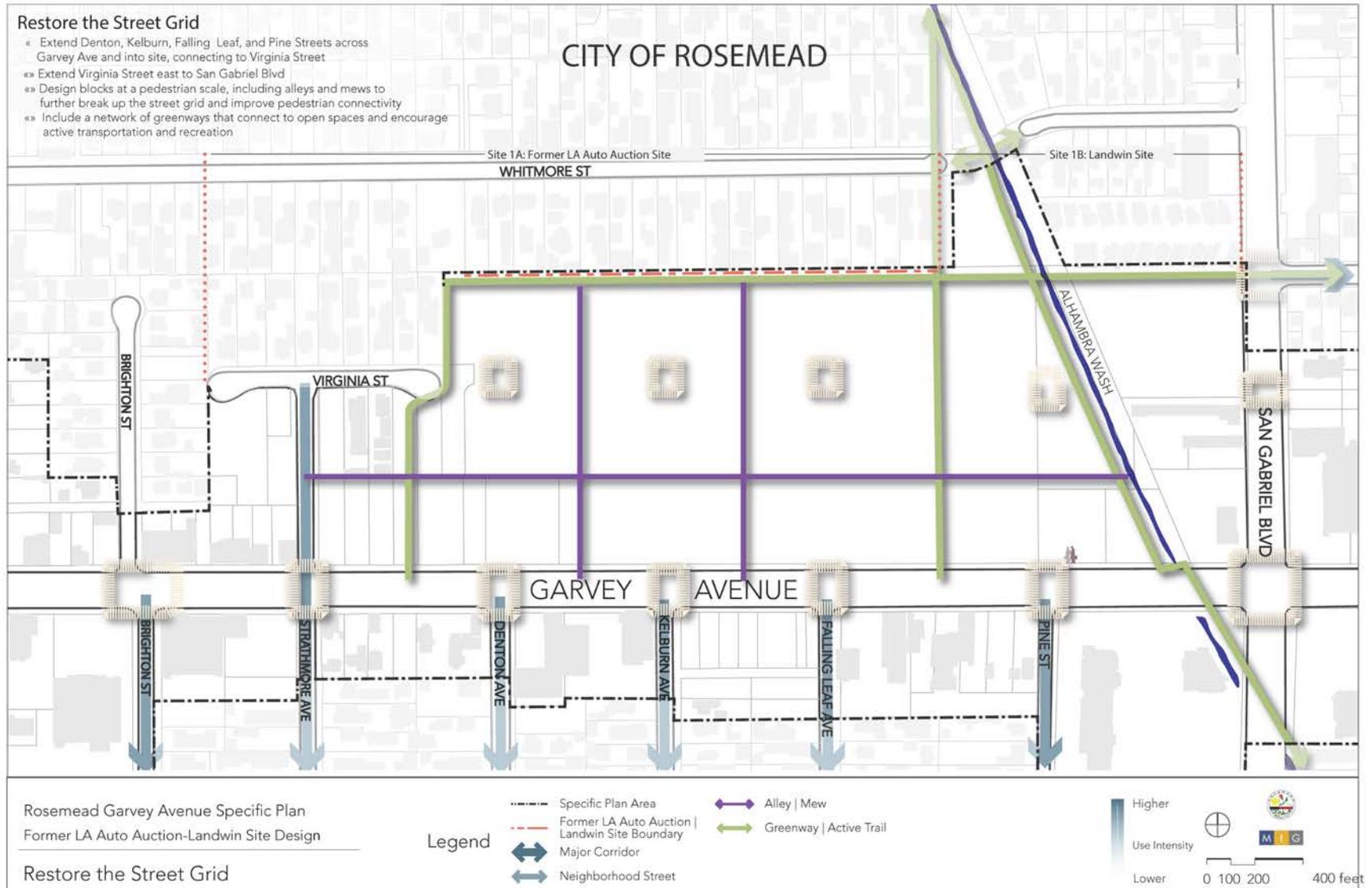


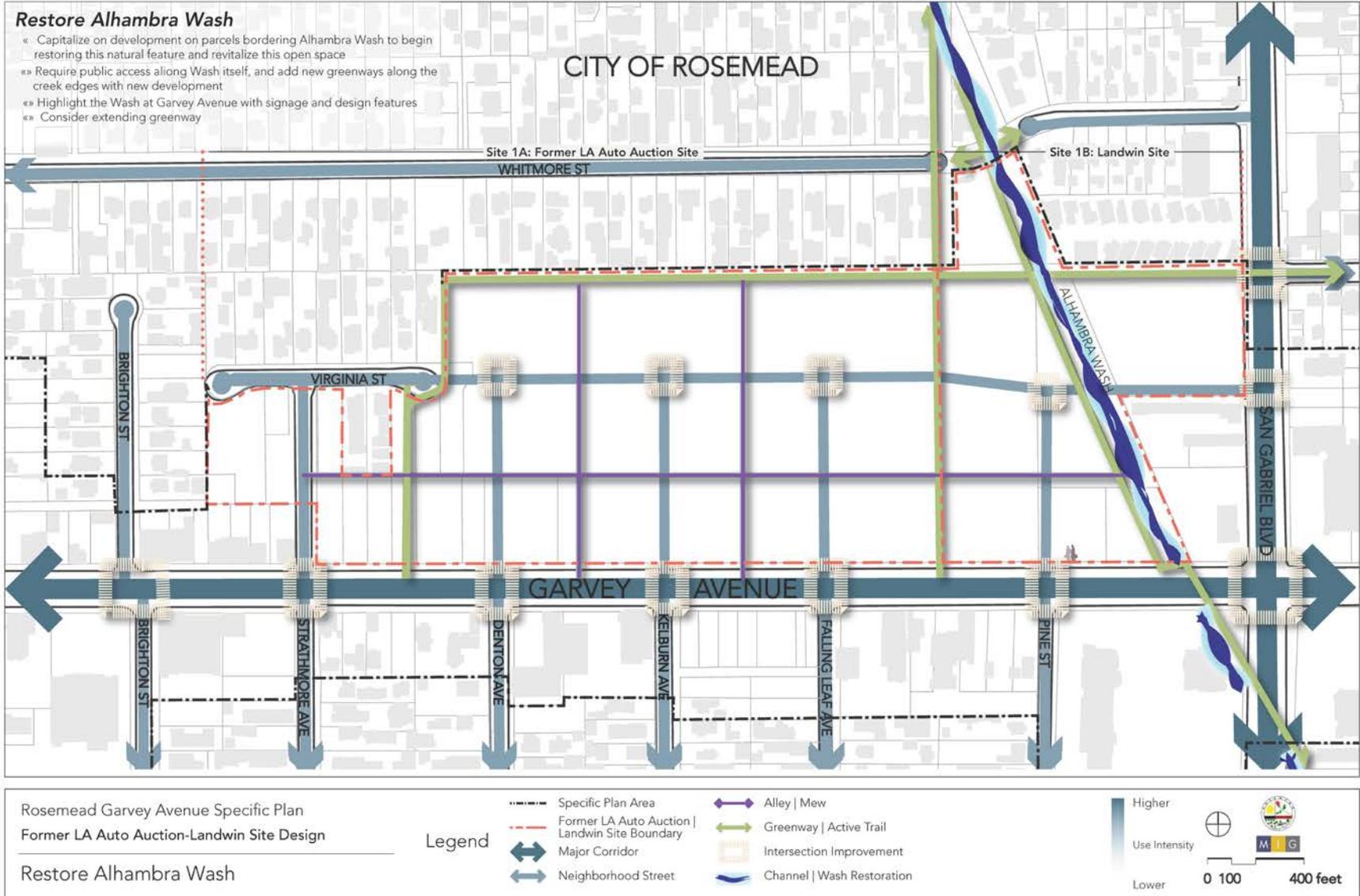
**This entry welcomes people with glass façade; nice example of onsite landscaping adjacent to the public realm.**

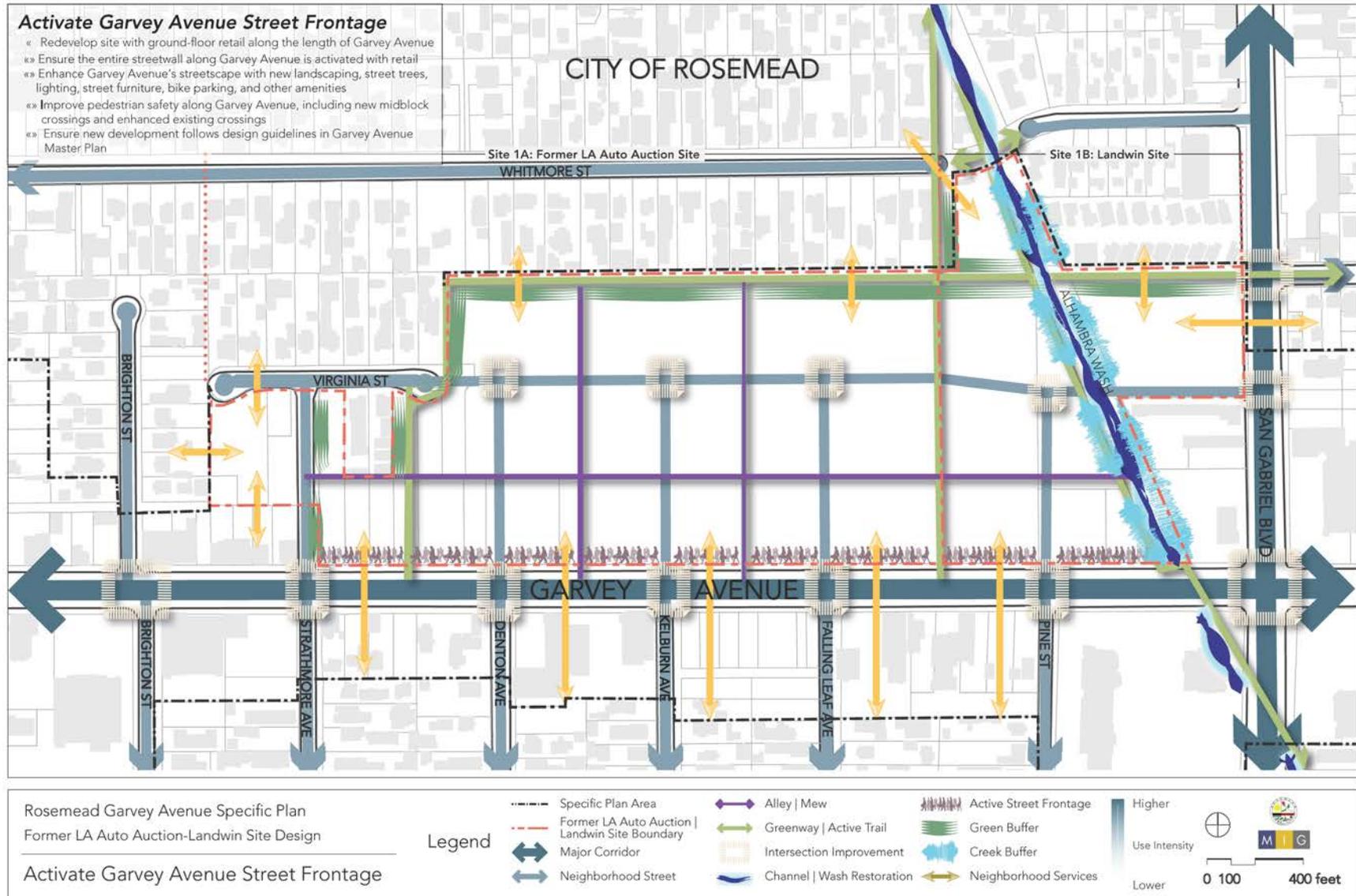
together, as one integrated site, even though the properties are under separate ownership.

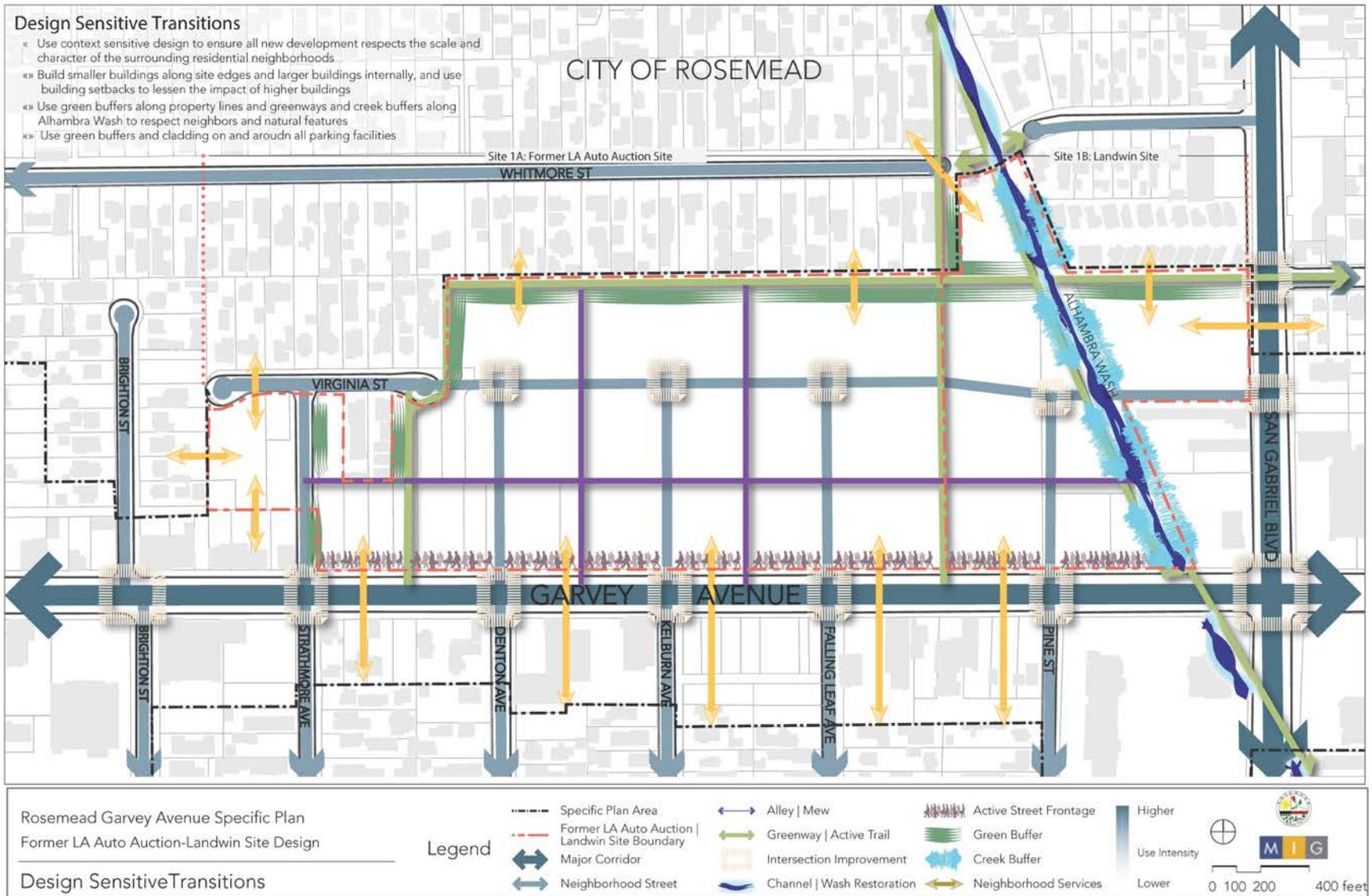
The design concepts expressed here are intended to serve as a best-practice design vision, meant to guide rather than be prescriptive, or to limit development to the concepts expressed in this Garvey Avenue Specific Plan. Alternatives may be put forth, provided they are consistent with the design principles and vision expressed here.

Figure 4.5 Diagram Series – LA Auto Auction and Landwin Property Recommended Site Design Principles



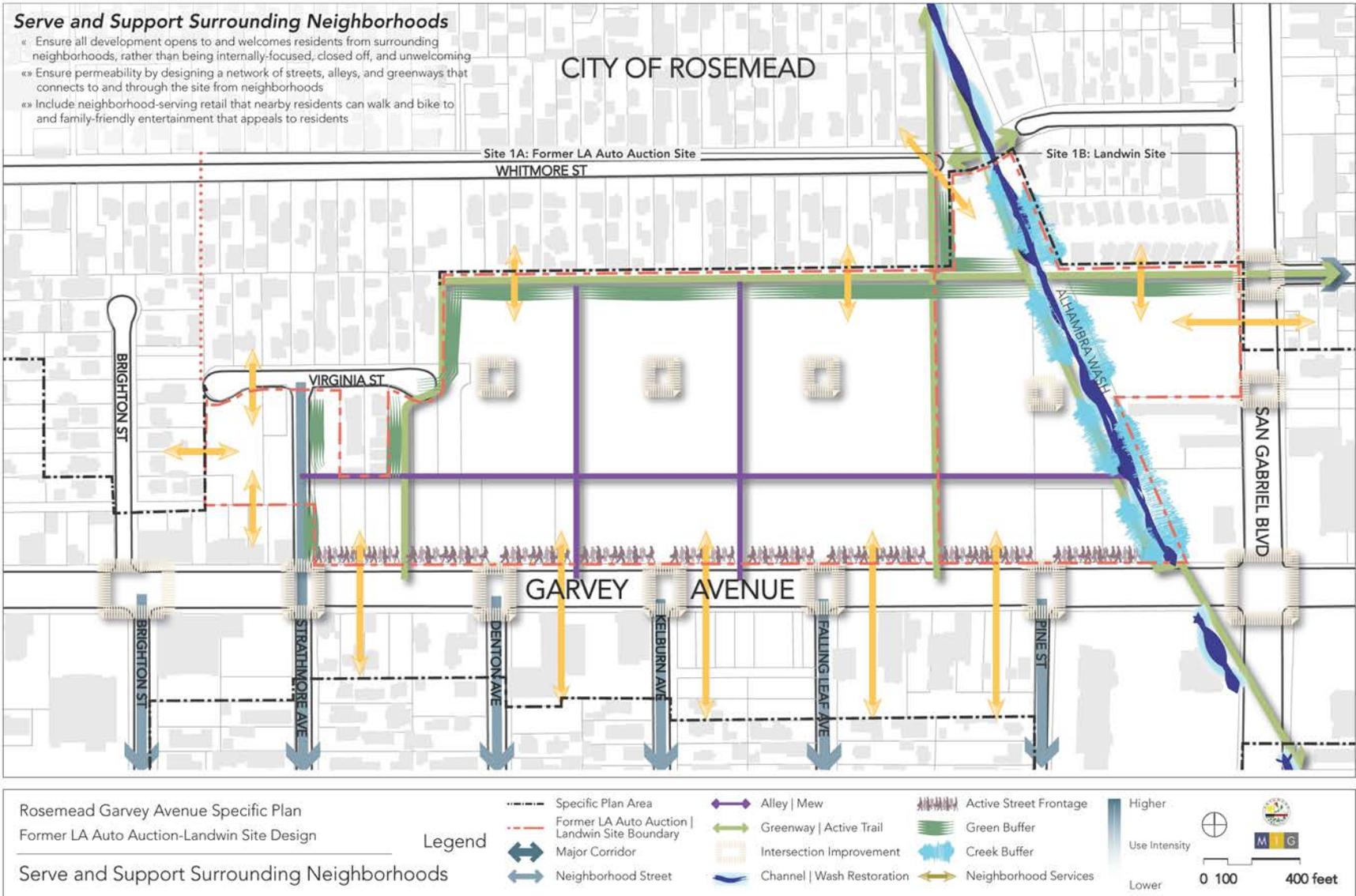


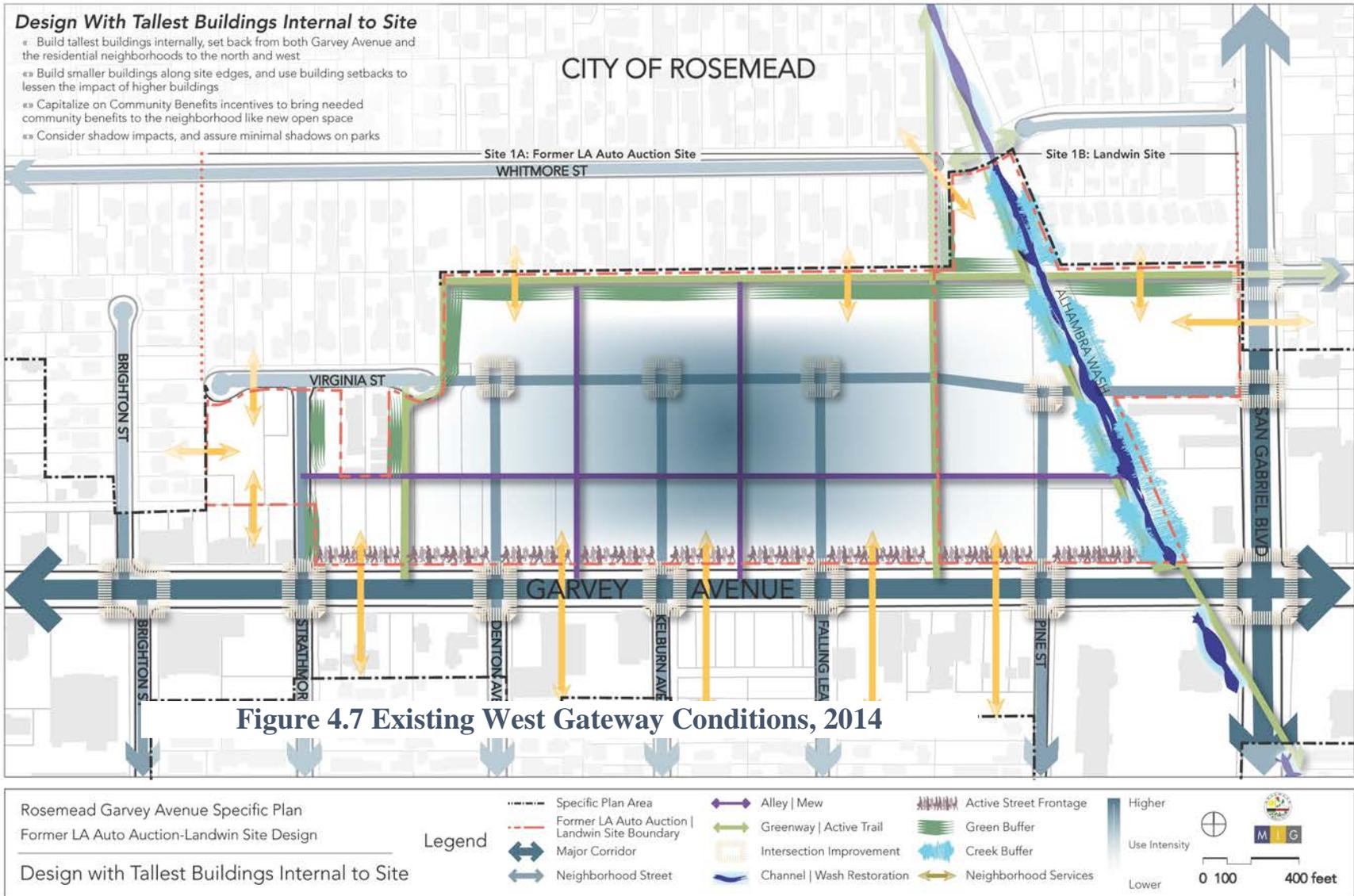




## Serve and Support Surrounding Neighborhoods

- « Ensure all development opens to and welcomes residents from surrounding neighborhoods, rather than being internally-focused, closed off, and unwelcoming
- « Ensure permeability by designing a network of streets, alleys, and greenways that connects to and through the site from neighborhoods
- « Include neighborhood-serving retail that nearby residents can walk and bike to and family-friendly entertainment that appeals to residents





The Garvey Avenue Specific Plan allows for the entire site to be developed with a range of mixed-use buildings of up to 75' in height, as well as a hotel. Both vertical (within one building) and horizontal (spread among several buildings) mixed-use would be allowed; the Garvey Avenue Specific Plan expects some mixed-use development to be horizontal, which would result in some buildings with only one use, such as major commercial or entertainment buildings or residential townhomes. Mixed-use buildings with ground-floor retail and offices or residences on higher floors would line Garvey Avenue. Retail commercial development would range from large restaurants to smaller specialty or neighborhood retail, as well as a hotel, movie theatres, active recreation facilities, and other family-oriented entertainment. These buildings could surround a public central square or park with a pedestrian promenade that served as the main central gathering place for the neighborhood. Outdoor dining and restaurants could line the edges of the central square.

This concept suggests two- to four-story townhomes would line the northern edge of the site, with private rear open space along a greenway buffer that would also

be an amenity for Whitmore Street residents whose rear yards would border the greenway. The greenway would be designed to connect with a new greenway trail along Alhambra Wash, and to Zapopan Park directly to the east, along Park Street. A network of calm, narrow streets, alleys, and greenways would connect the neighborhood to Garvey Avenue and to surrounding neighborhoods. The westernmost parcels allow for new lower density residential uses to be integrated into the existing neighborhood. All parking would be consolidated within or below buildings, and at the rear of the site, in a centralized garage structure with a green buffer for the existing single-family housing bordering the site's northern edge. While the development would have sufficient parking, residential parking permits could be considered for adjoining neighborhoods if spillover parking became a concern. Another new neighborhood park could buffer existing single-family housing along the site's west edge.

These two sites (L.A. Auto Auction and Landwin), and the Park Monterey Site (generally bounded by the western City boundary and Prospect Place, north of

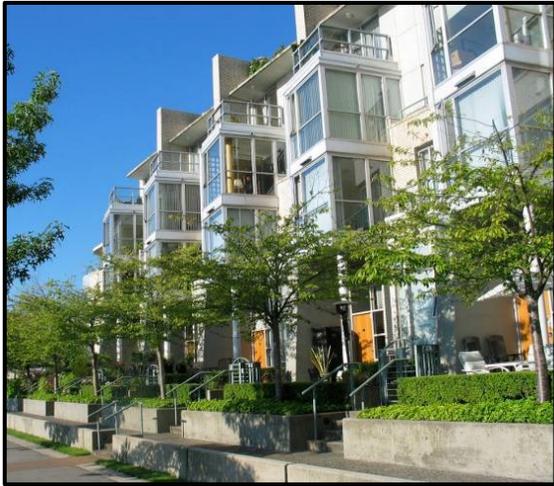


**Recycled water features have a calming effect in green space.**



**This storefront features integrated blade signs, awnings, and lighting.**

Garvey Ave), will be the major mixed-use sites within the Garvey Avenue Specific Plan. If a proposed project is submitted to



**Building projections and landscaped retaining walls create a layered effect.**

the City, which is within the Garvey Avenue Specific Plan, as may be the case with these sites, with a request to record more than one parcel map within the project site, the minimum parcel size will be one acre. Under this scenario the total project site will be evaluated by City staff, with the applicable FAR and density being taken into consideration. With the understanding that multiple parcels may be required in order to make the projects financially feasible to develop and parcels may be sold after approvals. With this scenario taken into consideration, it will be required to provide reciprocal agreements from each parcel in order to

provide easements, shared parking, building setbacks, required landscape areas, and other items to ensure cohesive and viable connectivity from one parcel to the next. This arrangement will be recorded as conditions of approval, set by the Community Development Director or Public Works Director.

The Landwin Property site, directly east, should be integrated seamlessly into any development of the former LA Auto Auction site. A hotel with approximately 150 rooms could anchor the Landwin Property site, along with additional mixed-use office-retail buildings. All would take advantage of a revitalized Alhambra Wash, its channel naturalized and its banks planted with trees and made green and accessible, and along which a new regional greenway would connect Garvey Avenue to the southeast with a new neighborhood park at Whitmore Avenue to the northwest. This new park would enlarge on and revitalize the existing public land and pedestrian bridge at Whitmore Avenue, and could serve as the anchor to a major new greenway for Rosemead. Townhomes at the northern edge of the site would help transition to the surrounding residential neighborhoods. The design concept for the

LA Auto Auction and Landwin Property sites is shown in Figure 4.6.

## 4.2. GARVEY AVENUE “WEST GATEWAY” SPECIALTY RETAIL DESTINATION

A vacant lot at 7430-7450 Garvey Avenue presents another catalytic development opportunity along Garvey Avenue in the western edge of the Specific Plan area – a new “West Gateway.” The parcel is vacant land surrounded by a chain link fence. Figure 4.7 shows the existing condition of the site, while Figure 4.8 shows the existing condition in a rendered perspective.

**Figure 4.6 Existing West Gateway Conditions, 2014**



Figure 4.7 LA Auto Auction and Landwin Site Conceptual Design Concept

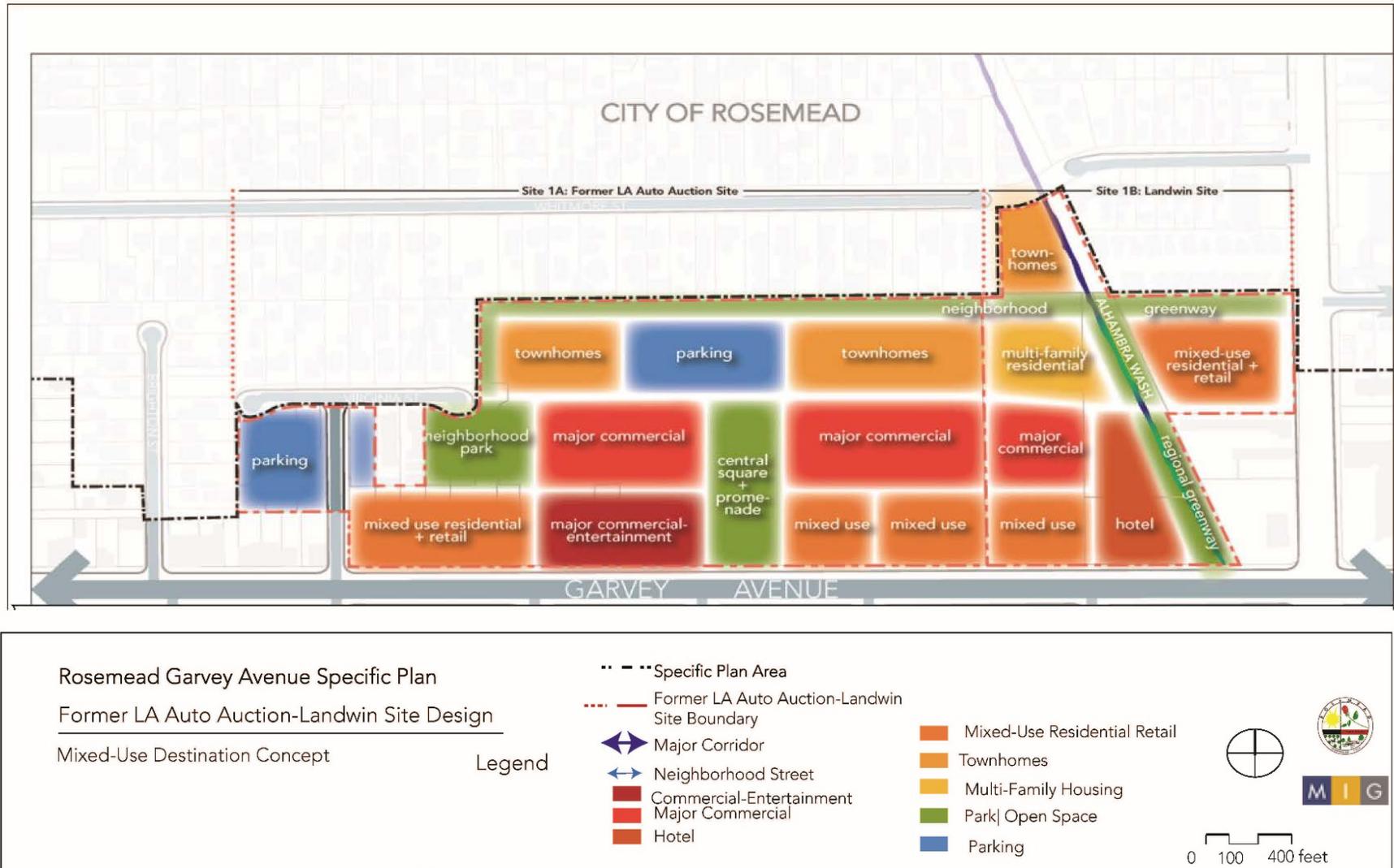


Figure 4.8 West Gateway Conditions, 2014



## Recommended Design Concept

The primary suggested design concept is an all-retail, pedestrian-friendly, small-scale mall with a central garden atrium and specialty retail shops. By fronting the street with public space and retail rather than parking, this represents a rethinking of the typical mini-mall, both enhancing the street with shops fronting onto a narrow gateway plaza on Garvey Avenue and offering an indoor garden experience for the public accessing the small-scale shops inside.

The building would be set back from the street, but rather than having parking in the front like a typical auto-oriented mini-mall, which degrades the street and pedestrian experience, the building would meet the street with a public plaza activated by the sidewalk-facing ground floor retail, with space for gathering and seating – and maintained by the property owner. The building could be two stories with one underground parking level, or three stories with two underground parking levels. The two-story alternative envisions specialty retail space in the front half of the ground floor and taking up the entire second floor, with parking in the rear of the ground floor and on one to three

underground levels. The maximum developable retail space would be 29,966 s.f., of which the majority would be on the second floor, with the remainder in the front half of the first floor. The building would have an FAR of 0.73.

The three-story alternative envisions a narrower building with retail space in the front half of the ground floor, and on the entire second and third floors; parking would be in the rear of the ground floor and on two to three underground levels. This alternative includes new public space and other environmentally sustainable building practices that would allow it to receive the Community Benefit Incentive and a maximum 1.0 FAR. The developable retail space would be 41,265 s.f.

In the two-story building scenario, from the rear 60' of the lot to the back of the property line would be used for access to the structured parking as well as a green buffer and setback space for existing residential development; in the three-story building, the rear 80' of the building would be used for parking. Figure 4.9 shows a rendered vision of the three-story West Gateway specialty retail destination.

An alternative design concept would develop the site with commercial development (Figure 4.10). This concept is intended to serve as a guide for an improved version of a specific kind of development that also occurs in places along the corridor. The development would front directly on the street, with active and lively ground floor retail and a strong pedestrian-oriented presence along Garvey Avenue.

The buildings could be two stories and would have pedestrian-friendly retail on the ground floor, and offices above. There would be 10,000 s.f. of both retail and office development. The FAR would be 0.59.

Small public plaza spaces or outdoor dining areas could be created in building setbacks, and pedestrian paseos could run between buildings.



Whimsical features add charm and character to the streetscape and plazas.

Figure 4.9 Suggested West Gateway Vision, Garden Mall Alternative



Figure 4.10 Suggested West Gateway Vision, Retail/Office Alternative



Parking could be located either in surface lot in the rear of the building, as is shown in Figure 4.10, or in an attached garage in the rear of the building on both levels, which could potentially allow for more of the site to be developed and would increase the amount of square footage and the FAR.

### 4.3. GARVEY AVENUE “PROTOTYPICAL DEVELOPMENT” OPPORTUNITY SITES

This concept is designed with flexibility in mind; it is an attempt to address the many parcels and small groups of parcels along the corridor that share a similar existing condition and would have similar development opportunities and constraints. The concept seeks specifically to address small (typically single or double) parcels with a marginal existing use. Typically, buildings are one-story, of low building quality and are sited well back from the street with parking in front. Figure 4.11 shows the existing conditions on a range of prototypical sites.

Numerous sites along the corridor make good candidates for prototypical

development. The type of development that could be achieved on each site depends on the width of each parcel, and on whether it reaches the side street to be a corner lot or is mid-block. The proposed development would work on some of the narrowest parcels on Garvey Avenue, which are typically 50’ or just under, and it would work on parcels that are two or more typical parcels in width, whether 100’, 150’, or 350’. Higher-quality and more cost-effective development could be built with wider parcels; combining parcels is encouraged, in line with similar recommendations in the Garvey Avenue Master Plan. Figure 4.12 shows potential locations within the Specific Plan area that are vacant or undeveloped and could be candidates for “prototypical development”. Figure 4.13 shows a rendered image of the typical existing conditions in a prototypical site.

The Specific Plan allows for a range of development concepts for prototypical sites ranging from a bare minimum and mostly cosmetic site remodel to more significant redevelopment that would develop much of the lot area, with setbacks and building modulation to transition to surrounding, lower-scale residential development. The concepts are

Remodel, Re-Use, and Redevelop. They are shown in a series of renderings in Figures 4.14 through 4.16, and detailed below.

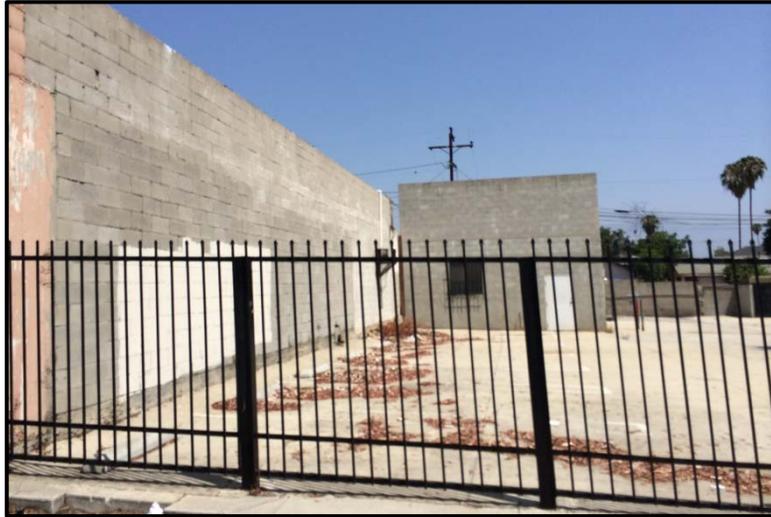
#### **Recommended Design Concept** Remodel

In this “light touch” alternative, existing development and site access would remain as is but building facades and site frontages will undergo a facelift, with significant landscaping improvements and streetscape enhancements to improve the pedestrian experience and bring all parcels up to a basic level of quality in terms of their interaction with the sidewalk and street.

#### Re-Use

In this alternative, some buildings could remain but the development level could be intensified by adding new buildings along the street wall, possibly with an accompanying change in land use (if located in a GSP-MU zone, for example). Streetscape and landscape design will be enhanced; vehicle access will be consolidated and improved for pedestrian

Figure 4.11 Prototypical Development Site Conditions, 2014



Empty lots are prime candidates for development.



This barren site property can be transformed with a catalytic development.

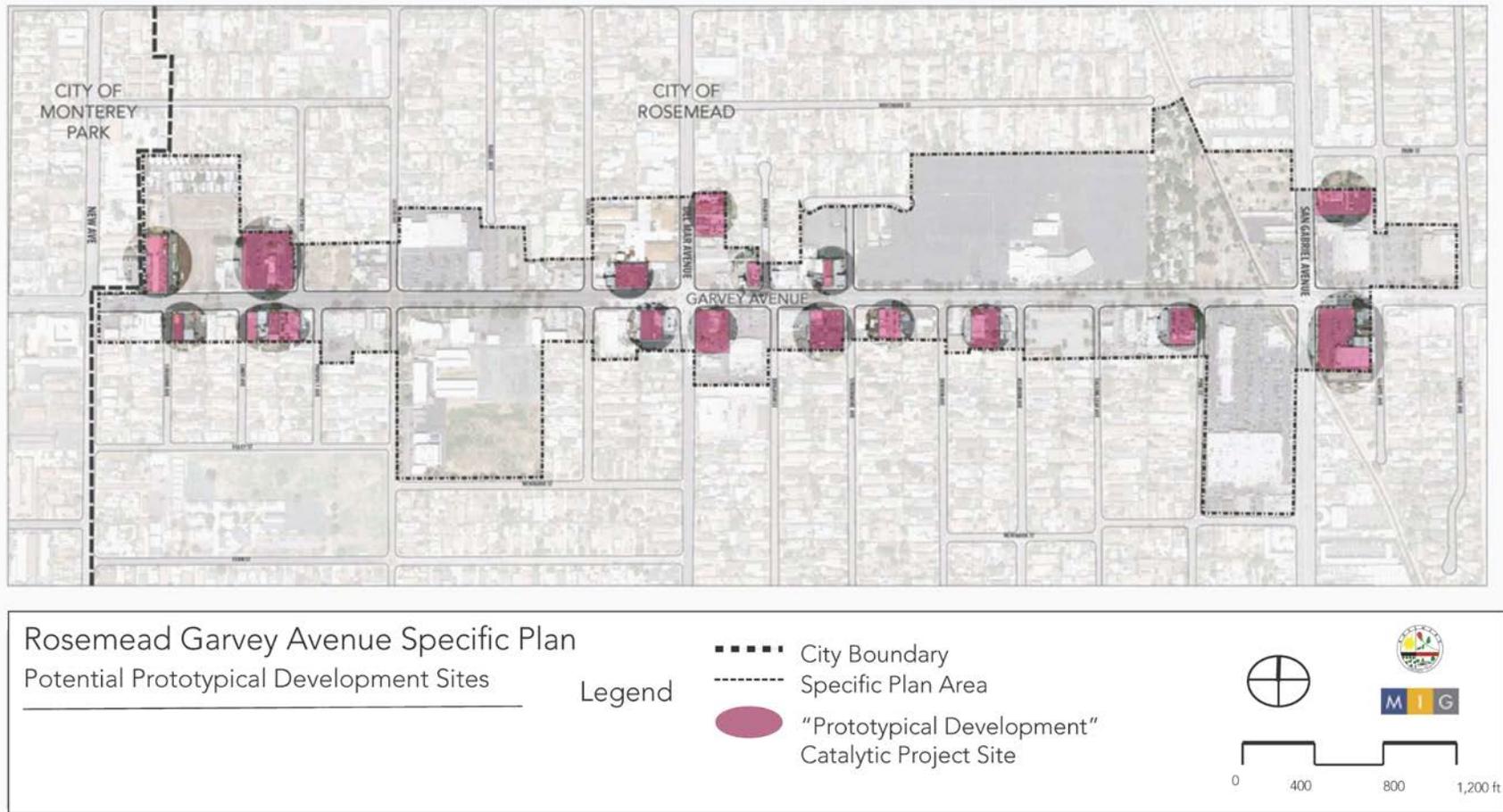


Infill development on parking lots is a possibility.



An example of underutilized development sites.

Figure 4.12 Potential Prototypical Development Locations Diagram



safety. Parking could be shared cooperatively for efficient land use.

Redevelop

In this alternative, existing buildings will be removed or, when possible, significantly remodeled, resulting in a higher intensity use (including, when appropriate, mixed-use) of two- to four-story buildings directly fronting the street, with parking in rear attached garages or underground. Active ground floor retail will replace surface parking lots; the sidewalk and streetscape along the parcel would be completely upgraded with pedestrian safety and comfort in mind. Several different types of full redevelopment are detailed below.

***Redevelopment Option: Two-story Commercial - Retail/Office (GSP).***

This option develops the site with a two-story retail and office building, with retail on the ground floor and office space above. Surface parking would be in the rear of the building. The building would be 27' high, the full width of the parcel, and 44' deep. The remaining distance to the back of the property line would be used for surface parking as well as a green

buffer and setback space. The FAR would be 0.59.

***Redevelopment Option: Two-story Mixed-Use Residential-Retail (GSP-MU).***

This alternative develops a prototypical development site with a two-story mixed-use residential-retail building, with surface parking in the rear of the building. The building would be the full width of the parcel and 55' deep. The remaining distance to the back of the property line would be used for surface parking as well as a green buffer and setback space. The density would be 17 dwelling units per acre.

***Redevelopment Option: Three- to four-story Mixed-Use Residential-Retail (GSP-MU).***

This alternative develops the site with a three- to four-story mixed-use residential-retail building, with two to three levels of structured parking in the rear of the building. The building would be the full width of the parcel and 110' deep. The remaining distance to the back of the property line (this is assumed to be 25', in a standard 135' deep parcel) would be used for parking access as well as a green buffer and setback space.

Figure 4.13 Prototypical Development 2014 Conditions



Figure 4.14 Prototypical Development Remodel Recommendation



Figure 4.15 Prototypical Development Re-Use Recommendation



Figure 4.16 Prototypical Development Redevelop Recommendation



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# Chapter 5

## INFRASTRUCTURE AND STREETSCAPE

### IN THIS CHAPTER

#### **5.1 Mobility and Streetscape Infrastructure**

#### **5.2 Infrastructure**

#### **5.3 Public Facilities**

This Infrastructure and Streetscape chapter identifies mobility, streetscape, public works infrastructure, and public services needed to serve the Garvey Avenue corridor. Key infrastructure along Garvey Avenue includes streets and sidewalks, sewer system, water supply system, and a storm drainage system. Public services include parks, schools, utilities, libraries, and emergency services.

Particular to the Garvey Avenue corridor, a redesigned streetscape (partially funded by and complementing the new private realm catalytic development) signals the dawn of a whole new street – a real place with a strong identity and gracious new community space that invites and prioritizes pedestrians and rapid transit users; with new bikeways that create safe, comfortable passage for bicyclists; and with cohesive new landscaping like new trees and sidewalk gardens, a green median, and green swales that manage stormwater – all of which help calm vehicle traffic, reduce noise, improve air quality, and provide shade and respite for a livable and appealing urban environment.

## 5.1. MOBILITY AND STREETScape INFRASTRUCTURE

Infrastructure for Garvey Avenue, a multimodal corridor, serves automobiles, bus transit, bicycles, and pedestrians. Garvey Avenue is a major arterial through Rosemead and the San Gabriel Valley. Garvey Avenue is a four-lane facility and often includes left turn and right turn lanes. On-street parallel parking is provided on both sides of the street throughout much of the corridor. A landscaped median is sporadically located along the length of the corridor. Sidewalks are provided on both sides of the street; however, formalized bike lanes are not.

From a transportation perspective, the Specific Plan is intended to balance the needs of the various roadway users.

The aim of the Specific Plan is to spur implementation of a full street redesign, whenever possible, to help the street become a more livable place and attractive destination with a striking design that fosters a strong pedestrian environment while serving a wide range of transportation.

### Streetscape and Roadway Configuration

Roadways traversing the Plan area include Garvey Avenue, New Avenue, Del Mar Avenue, and San Gabriel Boulevard. Garvey Avenue will be improved to include the provision of:

- Landscaped medians;
- Bikeways;
- On-street parking;
- Bus loading bays;
- Restriping to create additional through lanes, dual left turn lanes, and right turn pockets; and
- Traffic signal upgrades.

The proposed roadway lane configuration is shown in Figure 5.1. Garvey Avenue cross sections are illustrated in Figures 5.2 and 5.3. Right-of-way improvements are to occur within the existing right-of-way. These figures are to be utilized for potential scenarios of the roadway improvements. Funding and future development opportunities will dictate the allowed spacing, landscape areas, parking, and related configurations. The Public Works Director will make the final determinations as development and projects occur over the lifespan of the Specific Plan.

### Traffic Signal Recommendations

Traffic signals in the Garvey Avenue corridor affect the mobility of all travel modes and are important elements in maintaining a balance among the competing needs of the pedestrian, bicyclists, and vehicles. Garvey Avenue traffic signal locations include: New Avenue, Jackson Avenue, Del Mar Avenue, Kelburn Avenue, and San Gabriel Boulevard. This Plan anticipates that additional traffic signals may be necessary to facilitate the redevelopment of the L.A. Auto Auction and/or Landwin Property sites. Travel patterns along Garvey Avenue are anticipated to change over time. To address such changes, traffic signal operations need to be periodically adjusted to continue to adequately address mobility for all modes.

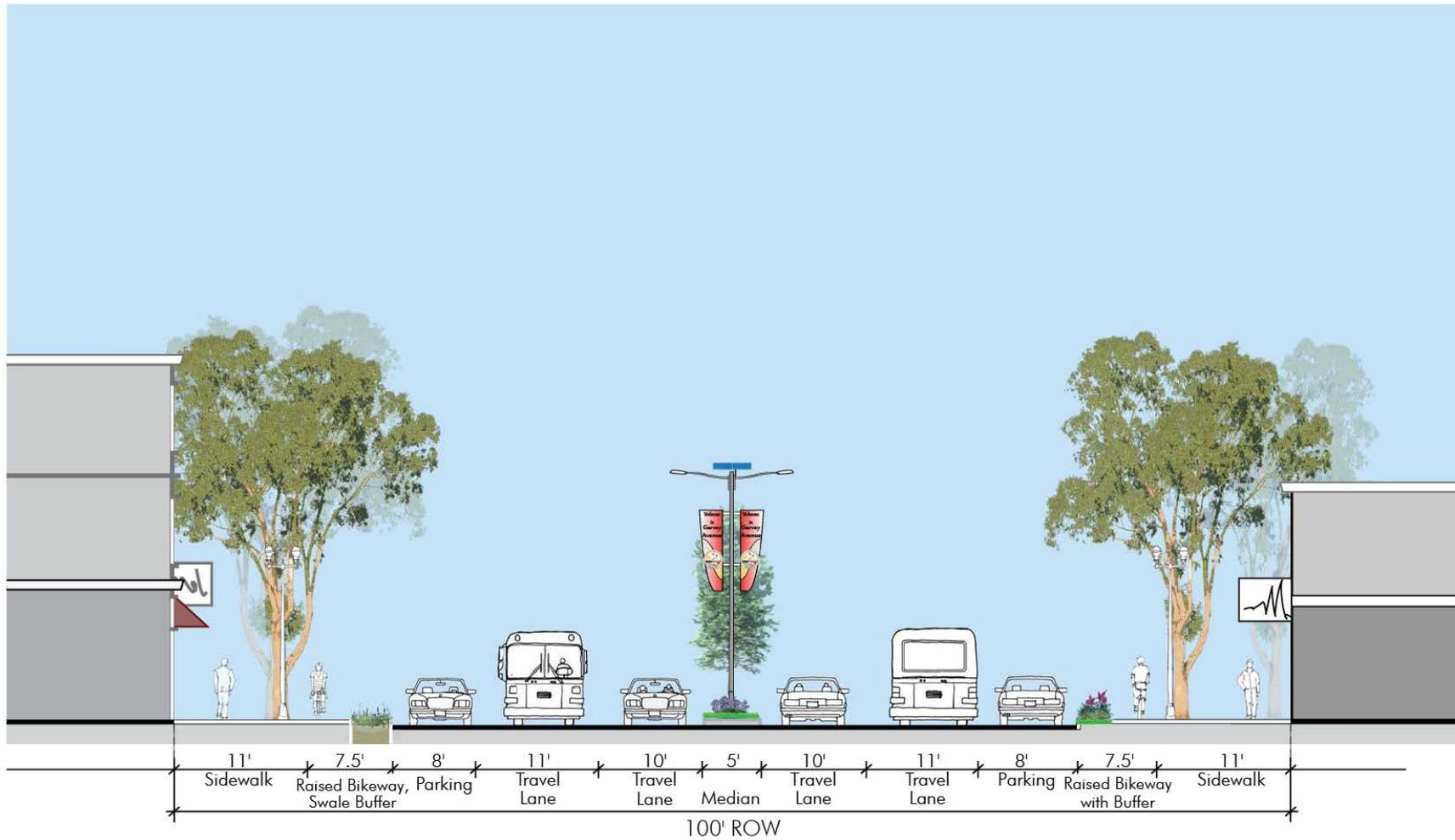
### Bus Transit

Bus transit provides a viable alternative to private automobiles. Bus transit occurring on Garvey Avenue includes Metro Lines 70 and 770 (Downtown Los Angeles to El Monte), Metro Line 176 (Highland Park to Montebello), Montebello Line 20 (San Gabriel to Montebello), and Rosemead Explorer Routes 1 and 2 (circular routes within Rosemead).

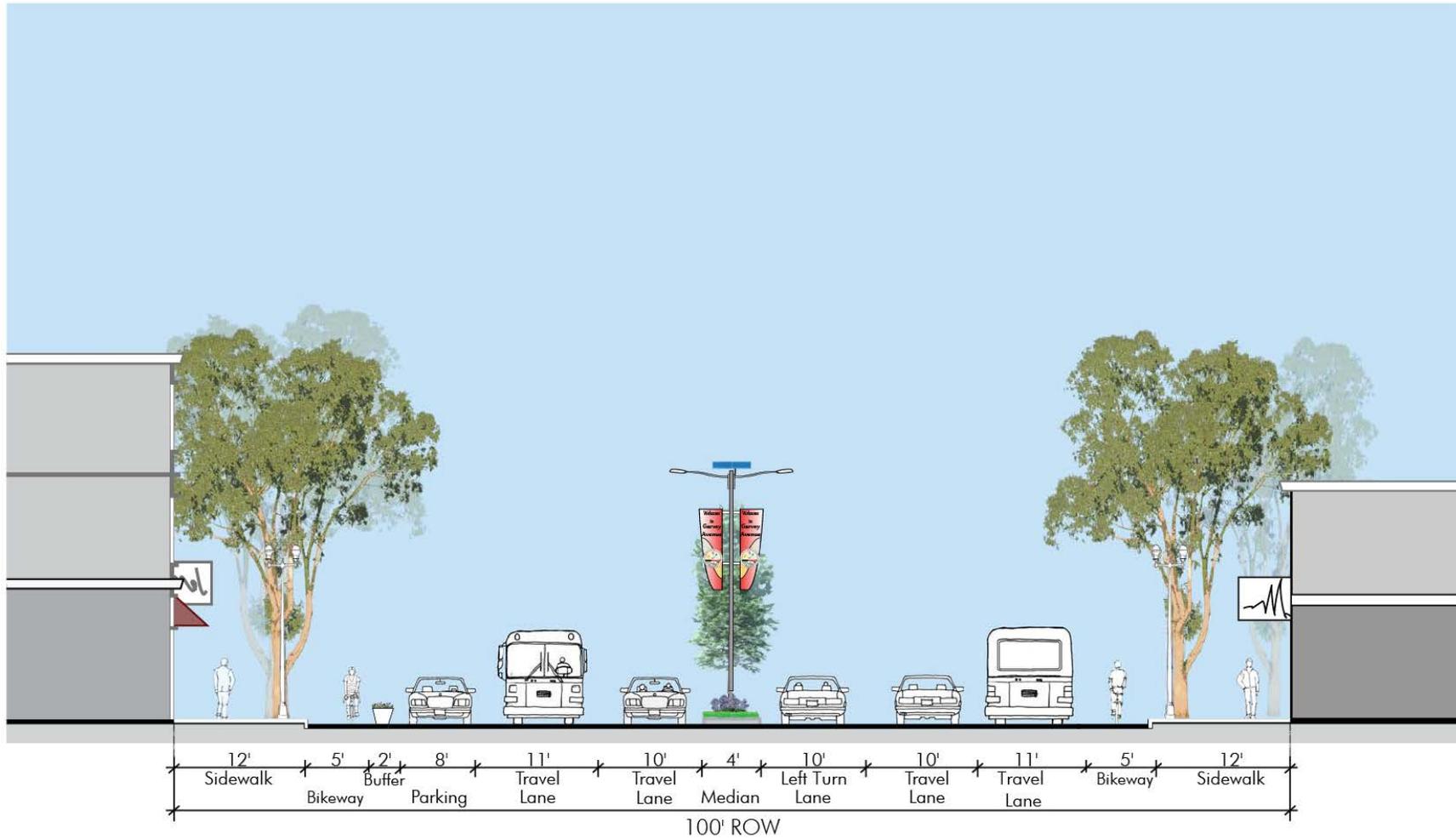
Figure 5.1 Garvey Avenue Lane Configuration and Streetscape Design



Figure 5.2 Proposed Garvey Avenue Section: Raised Bike Lanes with Parking and Median



**Figure 5.3 Proposed Garvey Avenue Section: Separated Bike Lanes with Parking, Reduced Median, and Left Turns**



The Specific Plan encourages the continued use and expansion of transit usage through the proposed land uses and their intensities and the inclusion of transit amenities within the proposed streetscape. Specifically, bus loading bays are identified (Figure 5.1); proposed amenities include improved bus shelters, benches, and trash receptacles.

### **Pedestrian Infrastructure, Streetscape, and Landscaping**

Garvey Avenue experiences different levels of traffic along its length. As such, it has differing mobility and pedestrian needs. To enhance the pedestrian experience along the corridor's length, especially as related to new uses, sidewalk widths at key intersections may be increased and enhanced with urban design

elements such as decorative crosswalks, landscaping, and street furniture. Additional marked crosswalks will enhance pedestrian connectivity along Garvey Avenue. Where crosswalks are provided, and as feasible, curb extensions into intersections may be used to enhance pedestrian safety. Where street blocks are long, midblock crossings can be considered.

Pedestrian-scaled street lamps should be provided throughout the corridor to enhance safety. Where possible, light standards shall provide both roadway lighting and sidewalk lighting.

New and infill street trees provide shade and identity to connect the corridor from end to end. Canopy and beautification

trees, such as the Fruitless Ginkgo Biloba (Maidenhair), Pink Trumpet (Tabebuia), White Crape Myrtle (Lagerstroemia, Natchez), and Brisbane Box (Tristania Conferta), will provide much needed shade as well as visual interest along the corridor. Street tree types and spacing must be approved by the City. Minimum tree size at the time of planting shall be 48" box and planted within a tree well with a tree grate covering the well. Parkways or swale buffers shall not be less than 3½' in width unless ADA requirements for minimum sidewalk width cannot be maintained. In this case, a parkway shall not be required. The parkways shall be planted in their entirety with small shrubs, groundcover, and water conserving plant materials.



Shaded bus stop amenities make transit usage more pleasant.



From upper left to right: Fruitless Ginkgo Biloba (Maidenhair), Brisbane Box (Tristania Conferta), Pink Trumpet, White Crape Myrtle (Lagerstroemia, Natchez)

## Bicycle Infrastructure, Streetscape, and Landscaping

To enhance bicyclist opportunities, a bikeway raised to the sidewalk grade will be considered throughout the corridor. The bikeway may descend to street level as the bikeways approach street intersections as needed to facilitate vehicle turning lanes. The bikeways, if constructed, should be separated from the sidewalks by a narrow planting strip or barrier that clearly delineates pedestrian and bicycle space.



Decorative crosswalks and midblock crossings delineate the pedestrian realm while providing a distinctive character to the corridor.

Implementation of the Specific Plan streetscape improvements will require planning, design, and funding. As such, short-term “quick fixes,” for a cost-effective streetscape redesign will provide

interim bicycle and streetscape improvements.

## 5.2. INFRASTRUCTURE

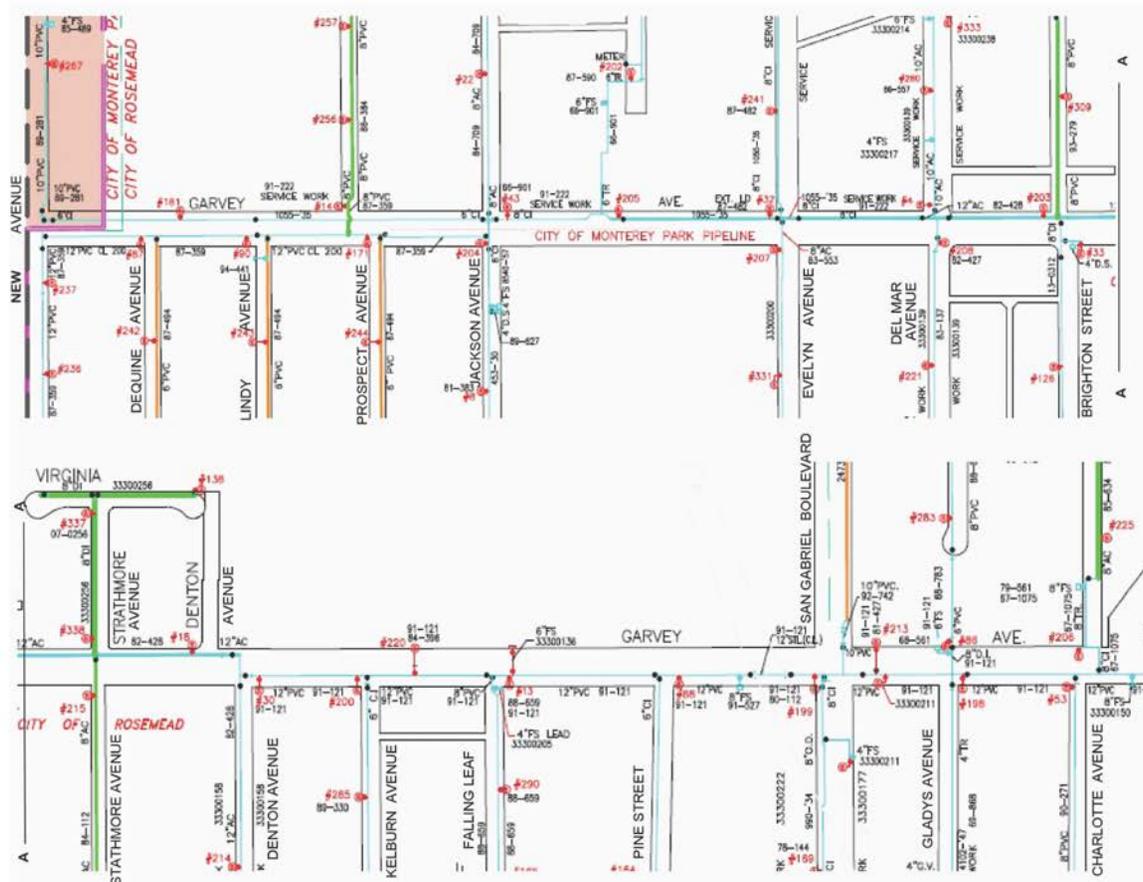
The Garvey Avenue corridor is largely built out and includes established infrastructure systems to support the existing population and businesses. Infrastructure systems include sewer, water, storm drainage, and solid waste disposal.

### Water

The City of Rosemead obtains water from six water suppliers but the Garvey Avenue Specific Plan area falls within the Golden State Water Company’s (GSWC) service area. The water system has the capacity to accommodate the Specific Plan’s anticipated development. Future water demands can be met until 2035 based on the land use assumptions for the Specific Plan area and the GSWC’s 2010 Urban Water Management Plan - South San Gabriel System.

The existing Garvey Avenue water line contains three segments of 6”, 8”, and 12” diameter water mains, see Figure 5.4.

Figure 5.4 Garvey Avenue Corridor Water Infrastructure



Rosemead Garvey Avenue Specific Plan  
 Garvey Avenue Corridor Water Infrastructure



Two existing 12” water mains are from Del Mar Avenue to Charlotte Avenue. San Gabriel Boulevard has a 10” water main north of Garvey Avenue and an 8” main south of Garvey Avenue.

All future development within the Specific Plan area will be assessed on a project by project basis for their fire-pump and fire-flow requirements.

## Sewer

The sewer system within the Plan area has existing capacity and structural deficiencies; a sewer capacity analysis based on an assumed General Plan buildout projection for the Garvey Avenue Specific Plan shows additional capacity issues.

The two sewer lines beneath Garvey Avenue have structural defects requiring pipe replacement; in particular, the segments between Brighton Street and Strathmore Avenue require immediate replacement. These upgrades could be pursued in conjunction with streetscape improvements or repaving. In addition, the 1996 City of Rosemead Sewer Study identifies existing sewer facilities capacity deficiencies. The “at capacity” pipes include those within the reaches along Del

Mar Avenue to the 36” trunk connection at Emerson Place (New Avenue/Del Mar Avenue study area); and reaches along San Gabriel Boulevard to the 21” trunk connection (San Gabriel Boulevard/Garvey Avenue study area). Further analysis is required as development is considered.

Most existing sewer pipes in the Specific Plan area are 8” diameter, with varied slopes along the flow path that connect to downstream sewer trunk connections, see Figure 5.5. There are five sewer trunk connections.

From New to Del Mar Avenues, two existing 8” sewer lines on both sides of Garvey Avenue flow downstream into a 36” trunk sewer. From Del Mar Avenue to San Gabriel Boulevard, two existing 8” sewers on both sides of Garvey Avenue flow downstream to a 27” trunk sewer. The sewers within these areas have adequate short-term capacity. Based on this Plan’s buildout assumptions, the downstream sewers may have inadequate capacity and may require improvement. As such, this Plan calls for additional infrastructure studies to determine specific infrastructure capacity requirements. The area along Garvey Avenue from San

Gabriel Boulevard to Charlotte Avenue is tributary to 8” sewer lines along San Gabriel Boulevard and Garvey Avenue; all flow is discharged downstream to a 21” trunk sewer. The areas to the north and south of Garvey Avenue have adequate capacities to the 8” sewer line along Garvey Avenue.

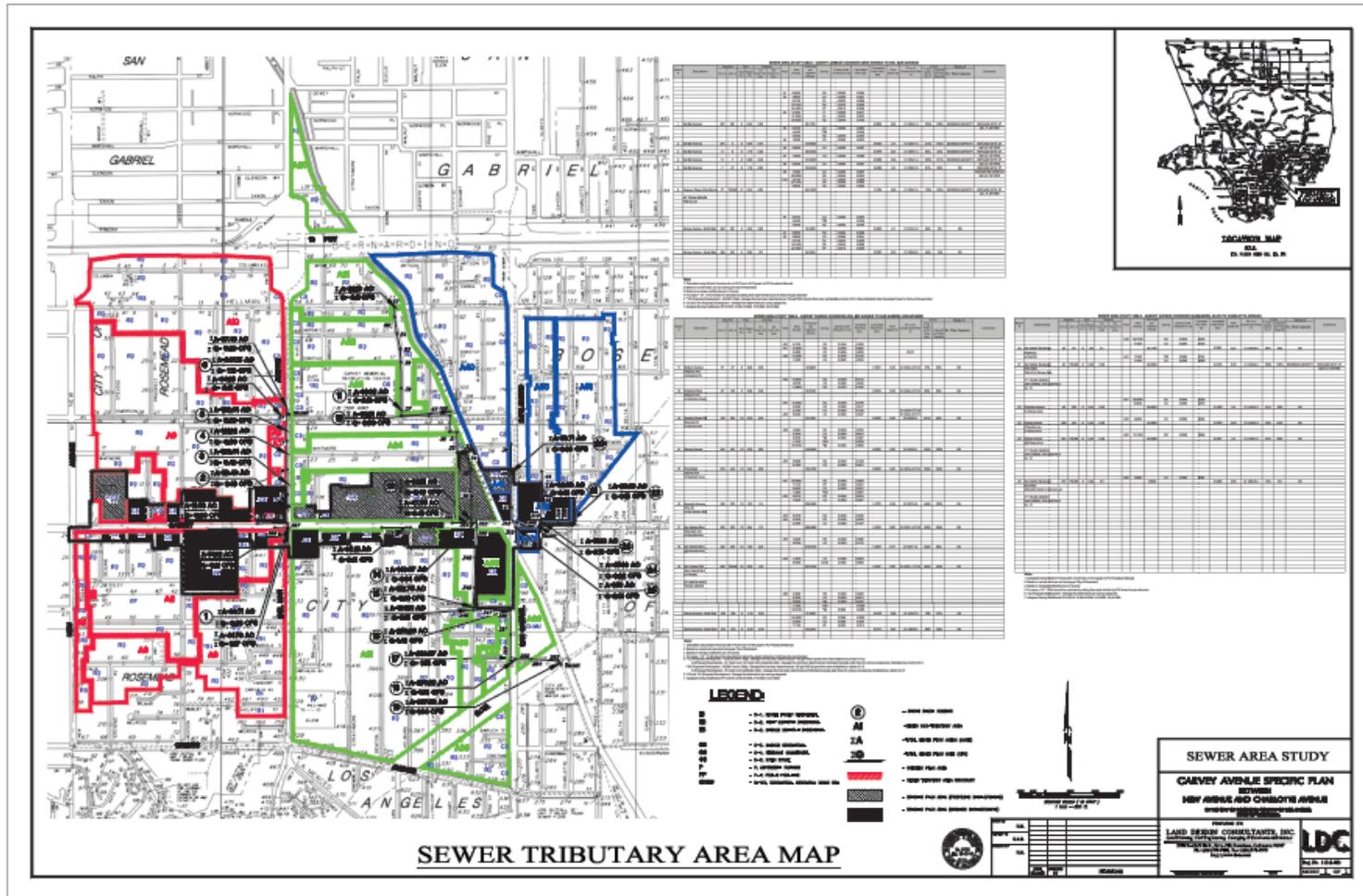
## Stormwater

The Garvey Avenue Specific Plan and all potential future development allowed by it will need to be assessed for its demand and potential impacts to the existing storm drainage system. Future development must minimize the amount of storm water runoff into the storm drainage system. The Garvey Avenue Streetscape Plan would manage as much of the street’s existing stormwater on-site as possible, using a variety of Low-Impact Development (LID) measures such as swales and rain gardens.

Any new stormwater infrastructure must be analyzed to meet all regulatory requirements, including but not limited to:

- Hydrology and Hydraulics Reports,
- Standard Urban Storm Water Mitigation Plan (SUSMP),

Figure 5.5 Sewer Tributary Area Map



Rosemead Garvey Avenue Specific Plan

Sewer Tributary Area Map

\* This graphic and corresponding data is available at City Hall.



Legend

- Sewer Tributary Area Boundary New to Del Mar Avenues
- Sewer Tributary Area Boundary Del Mar to San Gabriel Boulevard
- Sewer Tributary Area Boundary San Gabriel to Charlotte Avenue



- Low Impact Development (LID),
- Water Quality Management Plan (WQMP), and
- City and/or County standards such as on-site retention/detention.

The Garvey Avenue Specific Plan area's watershed drains into several drainage systems. The main drainage system is a reinforced concrete (RC) box culvert that runs along Garvey Avenue to the Alhambra Wash and several RC pipes drainage systems. Other storm drainage facilities include RC pipes and catch basins, depending on the section of Garvey Avenue, see Figures 5.6 - 5.8.

The drainage areas, north of Garvey Avenue from Brighton Street to the Alhambra Wash, and north of the Alhambra Wash and San Gabriel Boulevard, are collected and discharged to Alhambra Wash.

### **Solid Waste and Recycling**

While the Rosemead Department of Public Works is responsible for solid waste collection and disposal for all residential properties, it contracts with Republic Services to provide these services. The Department also oversees a curbside recycling program, which

includes collection of paper and cardboard, cans/aluminum, plastic, and glass, as well as green waste for grass, leaves, and plant trimmings. The City will continue to encourage solid waste reduction and recycling efforts including expanding public outreach programs focusing on recycling and composting education.

### **5.3. PUBLIC FACILITIES/ SERVICES**

Garvey Avenue has a handful of public facilities/services within and around the Specific Plan area. These include schools and institutions, utilities, and emergency services. The services provided by the public facilities help to provide a strong sense of community and place along Garvey Avenue.

#### **Parks**

Parks, recreational facilities, and open space are lacking along the corridor. The Specific Plan's open space requirements and the identified open space enhancements (such as those along Alhambra Wash) are anticipated to result in new development that provides additional park, recreation, and open

spaces for the corridor and nearby residents and visitors use and enjoyment.

#### **Schools**

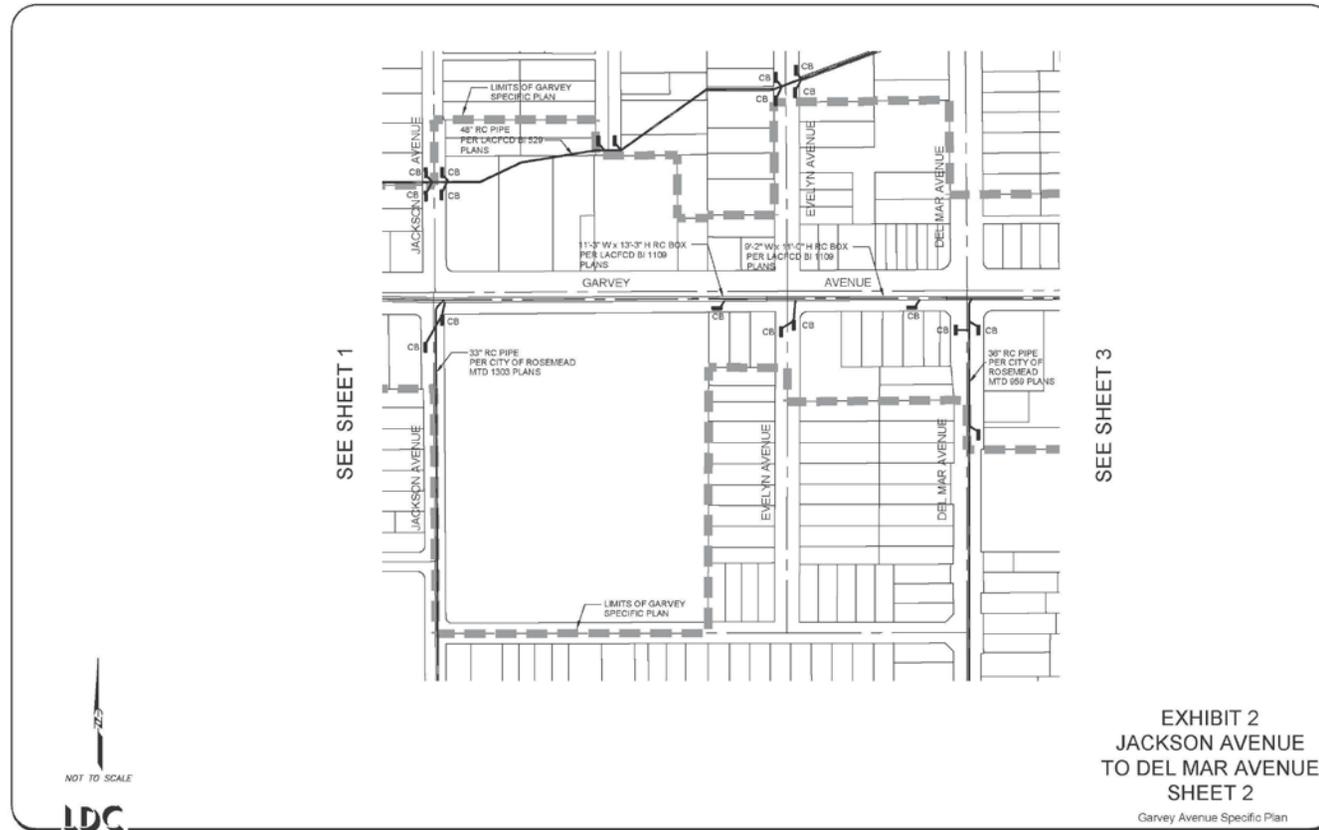
Improvements along the corridor are intended to help integrate Richard Garvey Intermediate School, a community anchor, into the neighborhood fabric. The anticipated development along Garvey Avenue is expected to result in an estimated 620 additional K-12 students (based on State of California generation rates).

#### **Utilities**

Electrical power within the Specific Plan area is provided by Southern California Edison. The company has a major power station due south of New Avenue, along SR 60. Power lines are underground along Garvey Avenue. Gas is provided by Southern California Gas, a subsidiary of Sempra Energy. The nearest major pipelines run underground along:

- Walnut Grove Avenue,
- Parallel to and east of San Gabriel Boulevard, and
- Fern Avenue, east of Walnut Grove Avenue.

Figure 5.6 Garvey Avenue Corridor Stormwater Infrastructure, New to Jackson Avenues



Rosemead Garvey Avenue Specific Plan  
 Stormwater Infrastructure Jackson to Del Mar Avenues

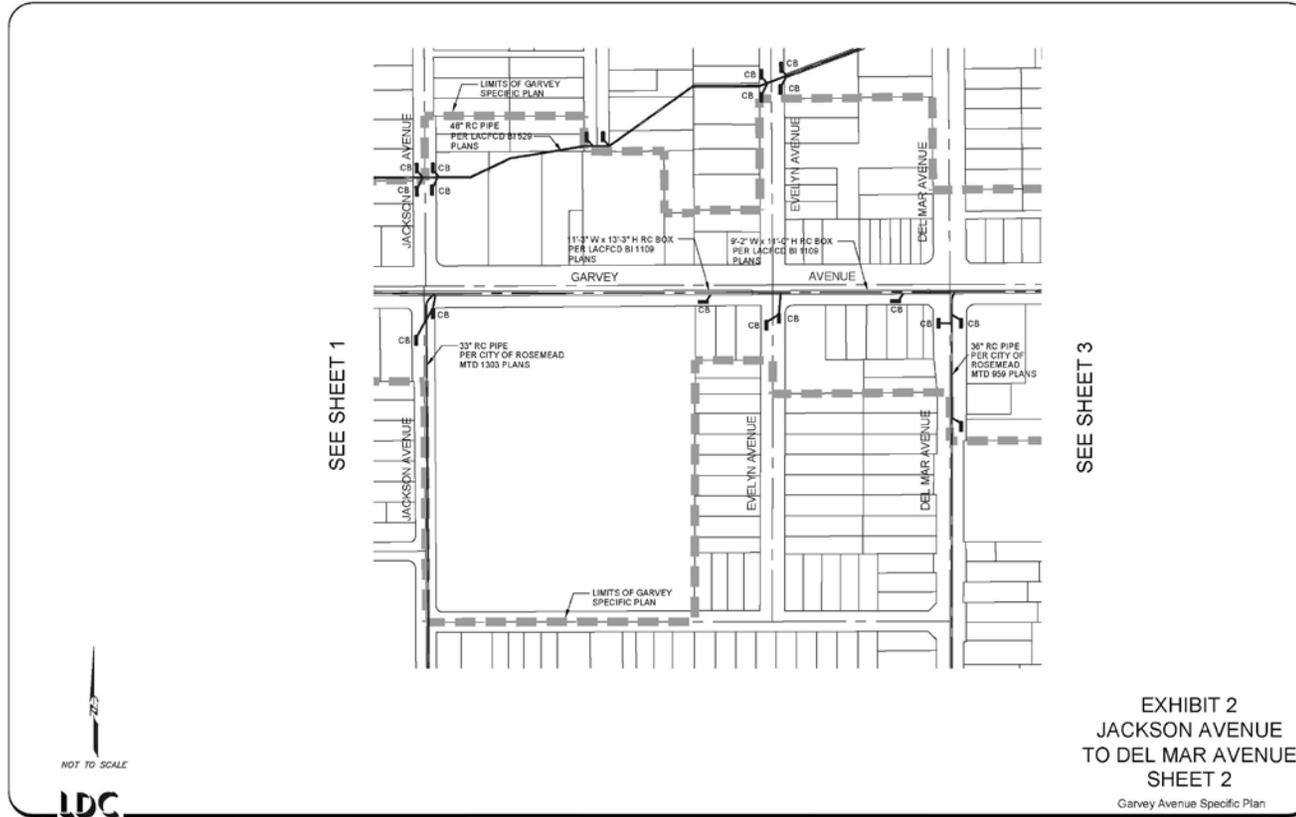
\* This graphic and corresponding data is available at City Hall.

Legend

-  Stormwater Box
-  Stormwater Piping
-  Specific Plan Area

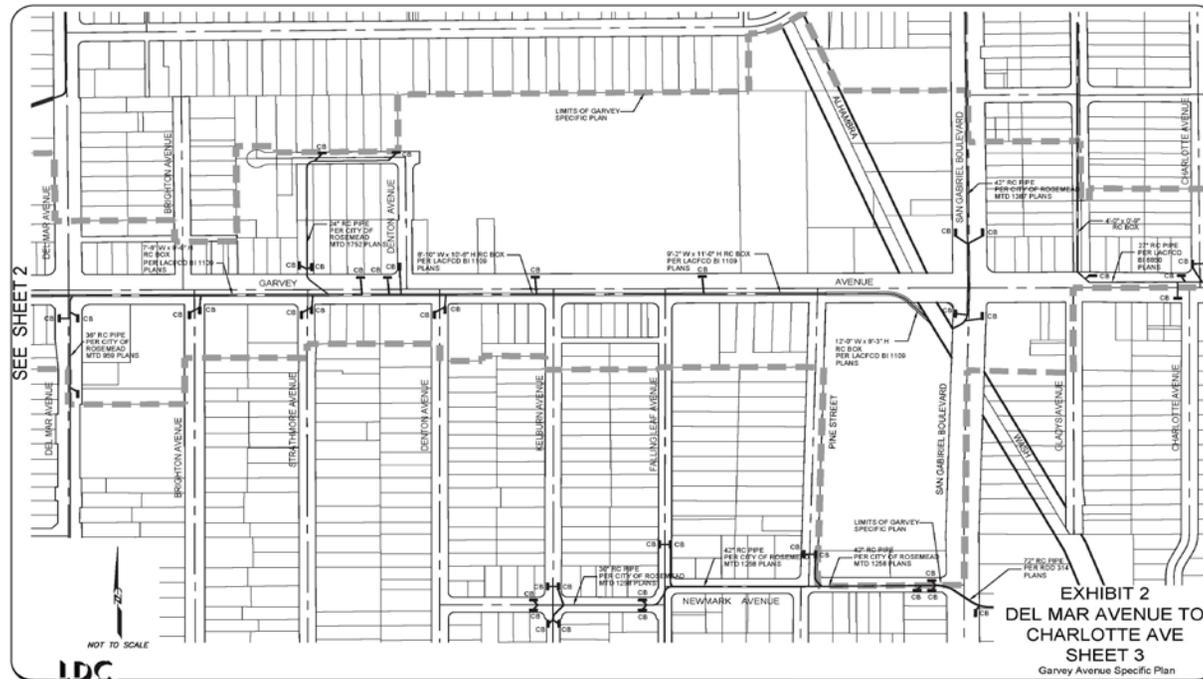


**Figure 5.7 Garvey Avenue Corridor Stormwater Infrastructure, Jackson to Del Mar Avenues**



<p>Rosemead Garvey Avenue Specific Plan</p> <hr/> <p>Stormwater Infrastructure Jackson to Del Mar Avenues</p>	<p>Legend</p> <ul style="list-style-type: none"> <li><span style="display: inline-block; width: 20px; height: 10px; background-color: black; margin-right: 5px;"></span> Stormwater Box</li> <li><span style="display: inline-block; width: 20px; border-bottom: 2px solid black; margin-right: 5px;"></span> Stormwater Piping</li> <li><span style="display: inline-block; width: 20px; border-bottom: 2px dashed black; margin-right: 5px;"></span> Specific Plan Area</li> </ul>		 
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Figure 5.8 Garvey Avenue Corridor Stormwater Infrastructure, Del Mar to Charlotte Avenues



Rosemead Garvey Avenue Specific Plan

Stormwater Infrastructure Del Mar to Charlotte Avenues

\* This graphic and corresponding data is available at City Hall.

Legend

Legend

-  Stormwater Box
-  Stormwater Piping
-  Specific Plan Area



Charter Communications is the primary cable provider for Rosemead, while AT&T is the primary provider of home, business phone, and internet services.

### **Emergency Services**

Fire and emergency medical services are provided by the Los Angeles County Fire Department, which has local stations on Valley Boulevard and San Gabriel Boulevard in Rosemead. Rosemead's police service is provided through the Los Angeles County Sheriff's Temple Station, in Temple City. Deputies assigned to Rosemead work out of Rosemead's Public Safety Center located on Garvey Avenue next to Zapopan Park at Charlotte Avenue. The Center is managed by Rosemead's City Manager's Office; it includes local offices for the Los Angeles County Sheriff, the Los Angeles County Probation Department, and community safety services.



# Chapter 6

## IMPLEMENTATION

### IN THIS CHAPTER

#### **6.1 Financing**

#### **6.2 Garvey Avenue Specific Plan Administration and General Provisions**

The Garvey Avenue Specific Plan identifies the location and type of desired development along Garvey Avenue, consistent with the City of Rosemead’s General Plan. Achieving this vision requires a concerted and cooperative public/private partnership, including significant capital investment. This chapter outlines the “how to” steps necessary to create positive change along Garvey Avenue in the short and long terms. This chapter also establishes the basis for the Plan’s administration. This chapter creates a solid foundation, from which the City can support the growth of existing businesses, attract new businesses and investment, allocate limited resources, and foster collaboration through strong public-private partnerships. While the Specific Plan remains visionary in its ambition, the action steps put forth a realistic and practical approach to assist economic growth, promote the “greening” of the neighborhood, improve walkability and bikeability, and ensure a successful, vibrant future for the Garvey Avenue Specific Plan area.

## 6.1. FINANCING

Development of the Garvey Avenue Specific Plan area will be primarily privately financed by the developers of individual projects. The following policies shall govern the funding of infrastructure, community facilities, and public services for the Garvey Avenue Specific Plan area:

- The infrastructure projects' costs will be allocated to future new development projects within the Plan area based on benefit received, with appropriate consideration of the potential impacts on the financial feasibility of proposed projects;
- All new development will pay the full cost of infrastructure needed to serve future private development projects within the Plan area;

- Private development will also fund the costs of mitigating any adverse impacts of future projects on the City's existing infrastructure;
- As a primary means of ensuring that new development pays its "fair share" of future infrastructure costs, the City established a "Development Impact Fee (DIF)"<sup>1</sup> that fairly allocates the cost of public improvements needed to serve new development in the Plan area. The City is currently in the process of determining the specific DIF related to the sewer system serving the Garvey Avenue Specific Plan. This fee will be adopted by Resolution at a later date. In addition, if there are any other fee(s) determined necessary because of the Garvey Avenue Specific Plan, then they will also be adopted by Resolution;
- Developers may proceed ahead of the City's streetscape and infrastructure plans if they pay the cost of extending the core infrastructure to their project(s),

subject to future reimbursement; and

- The impact fee structure will include a method for future reimbursement of landowners who opt to provide up-front financing of improvements benefitting other properties that develop later.

The construction of water, sewer, drainage, street, and streetscape improvements shall either be completed by the utilities, the City of Rosemead, or under the direct direction of these agencies. All water, sewer, drainage, street, and streetscape facilities will continue to be maintained by the agencies or their appointed designees.

Whenever possible, pay-as-you-go financing shall be used. Debt financing should only be used when it is essential to provide facilities necessary to permit development or to maintain service standards.

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<sup>1</sup> The City has recently adopted its development impact fee schedule for Citywide fees; implementation of additional fees unique to the Garvey Avenue Specific Plan area would require a nexus study quantifying future infrastructure costs and allocating them by future land use.

While infrastructure improvements shall be, primarily, the responsibility of the private development community, some portion may be publicly financed subject to available funding sources. For these public expenditures, several grant, loan, and value capture funding mechanisms (as outlined later in this chapter) can be used to finance the infrastructure and other community improvements.

## **Local Tax Increment and Assessment Districts**

### Assessment Districts

Assessment districts provide a mechanism for property owners to choose to levy an additional tax upon themselves for identified purposes. California law allows the creation of assessment districts for a wide variety of purposes; these can either fund capital improvements or be established for operating costs (such as lighting and landscaping districts). Specific districts that might be applied to the Garvey Avenue Specific Plan would be a parking district and/or streetscape improvement district.

Two primary challenges in establishing assessment districts are: property taxes can only increase a certain amount before new development is disadvantaged relative to properties not subject to an assessment; and assessment districts require a two-thirds majority vote of property owners weighted by property value to pass. In an area with numerous small properties, achieving the approval of two-thirds of the voting properties would likely be difficult.

### Reimbursement District

The City can form a Reimbursement District when one party pays for and constructs or causes to construct infrastructure that has the potential to benefit another party. If during the life of the reimbursement district (typically 10-20 years), the benefitted property owner makes use of the improvements, then the reimbursement is due. The process includes preparation of a report that outlines the improvement costs, the method of distributing the costs, the properties involved and includes details of the reimbursement agreement. The City Council will convene a public

hearing and district property owners can provide input. Once approved by the City Council, a resolution is recorded against properties in the district. Reimbursements are collected when properties in the district develop or make use of the improvements.

### Capital Improvement Program (CIP)

Capital Improvement Program is a comprehensive analysis of needs and available financing for all capital improvements in the City. The CIP is updated every five years and implemented on an annual basis.

## **Other Local Sources of Funds**

### Impact Fees

Impact fee programs can be a funding source for transportation improvements, sewer or storm drainage infrastructure, streetscape, parks, and recreational open space. These fees, paid by new residential and commercial development projects, must only be used to pay for improvements that can be demonstrated to serve new residents and new businesses. These fees can be combined with other funding sources to

fund a project that serves both new and existing residents or businesses. A nexus study, which calculates the new increment of development, estimates the portion of an improvement project attributable to that increment of growth, and allocates the fee among the new development projects by land use, is required by state law for implementation.

#### Revenue Bonds

Revenue generating public activities that create sufficient cash flow to cover operating costs and debt service can potentially issue municipal debt to cover the cost of capital improvements. A common example of this is revenue bonds for parking garage construction where there is pay parking.

#### Other Public Debt

New commercial and lodging projects could generate significant new sales tax and transient occupancy (lodging) tax revenues that will flow into the City's General Fund. This new money could be used to finance debt service on tax-exempt debt obligations so that existing

activities provided through the General Fund are not impacted.

#### “Adopt A Light” Program (Tree, Bench, etc.)

As a unique method for paying for street light fixture other streetscape elements, a small projected plaque sign could be affixed to a light pole with the name or logo of the local merchant, business, person, entity “purchasing” the fixture.

#### Development Agreements

A Development Agreement can be an effective tool encouraging public-private partnership in large scale projects that are developed in series of phases. It provides developers with a specific level of certainty about the land use requirements and gives the City an opportunity to advance its local planning policies through comprehensive planning efforts. Benefits to both public and private parties include: greater latitude in enforcing requirements and contingencies on proposed developments; and developers have a level of certainty about land use and how the regulatory standards will not

materially affect later development phases once the project has been approved.

#### State Sources

##### State Transportation Improvement Program (STIP)

The STIP is a multi-year capital improvement program of transportation projects on and off the State Highway System, funded with revenues from the State's Transportation Investment Fund and other funding sources, including the State Highway Account. A wide variety of transportation capital projects are eligible for funding, including improvements to State highways and local roads, public transit (including buses), intercity rail, pedestrian and bicycle facilities, and inter-modal facilities. STIP programming generally occurs every two years, with the California Transportation Commission (CTC) adopting a fund estimate in August of odd years. Transportation improvement plans prepared by Caltrans and local agencies are then submitted to CTC for approval by December of odd years. Caltrans

prepares the Interregional Transportation Improvement Plan, which governs roughly 25 percent of allocated funds for intercity projects. Regional Transportation Improvement Plans are prepared by regional agencies including Regional Transportation Planning Agencies, County Transportation Commissions, and Metropolitan Planning Organizations.

### Caltrans Planning Grants

Caltrans provides planning grants for studies for sustainable transportation and transit planning studies, which can include studies that lead to SB 375 SCS implementation, corridor studies, evaluations of transportation issues involving intermodal facilities, and complete streets studies, among other planning activities.

Caltrans also provides grants for infrastructure projects that benefit bicycle commuters through its Bicycle Transportation Account. The project must increase the safety and convenience of bicycle commuters.

### Infrastructure State Revolving Loan Fund (ISRF)

The California Infrastructure and Economic Development Bank (I-Bank) loans money for infrastructure projects around the State. The I-Bank is the state's general-purpose financing authority that finances public infrastructure and private development projects that promote economic development and revitalize communities.

### **Federal Sources**

#### Moving Ahead for Progress in the 21st Century (MPA-21)

Signed into law in 2012, the Moving Ahead for Progress in the 21st Century Act (MAP-21) is the nation's current long-term transportation authorization. Map-21 replaces SAFETEA-LU, the authorizing legislation in effect from 2005 to 2012, though it continues or restructures many of the funding programs under the former legislation. MAP-21 authorizes \$105 billion for fiscal years (FY) 2013-14 and 2014-15 to be distributed by the Federal Highway Administration (FHWA) and

Federal Transit Administration (FTA) through a series of competitive grant and financial assistance programs for highway and road, transit, freight, bike, pedestrian, and multimodal projects. Programs administered under MAP-21 include the Transportation Alternatives Program (TAP). TAP, \$819.9 million in FY 2015-16, is to provide for a variety of alternative transportation programs, including bike and pedestrian trails and infrastructure-related projects for non-drivers. TAP consolidates funding from the former Transportation Enhancements, Recreational Trails, and Safe Routes to Schools programs.

## 6.2. SPECIFIC PLAN ADMINISTRATION AND GENERAL PROVISIONS

### **Applicability**

The Garvey Avenue Specific Plan serves as the implementation tool for the General Plan.

The development standards and guidelines contained in this Specific Plan provide for land development and use within the Specific Plan area. The

Specific Plan supersedes the otherwise applicable City of Rosemead development standards and regulations unless stated otherwise in this document. Whenever the provisions and development standards contained in this Specific Plan conflict with those contained in the City of Rosemead Municipal Code, the provisions of the Specific Plan shall take precedence. Where the Specific Plan is silent, the City of Rosemead Municipal Code shall apply.

### **Interpretation**

All interpretation of the provisions of this Specific Plan shall be made by the Community Development Director, unless stated otherwise in this document, RMC Section 17.40.040.

If an issue, condition, or situation occurs that is not sufficiently covered or provided for in this Specific Plan, those that are applicable for the most similar issue, condition, or situation shall be used. Unless otherwise provided, any ambiguity concerning the content or application of the Specific Plan is resolved by the Community

Development Director in a manner consistent with the vision, goals, objectives, and intent established in the Garvey Avenue Specific Plan.

### **Severability**

If any provision or portions of any provisions of this Specific Plan or its application to any person or circumstance are held to be invalid, the remainder of this Specific Plan and the application of those provisions to other persons or circumstances shall not be affected.

### **Required Actions and Entitlements**

#### City of Rosemead Plan Review

The City of Rosemead planning, building and safety, public works, recreation and parks, public safety, Los Angeles County Fire Department, and other staff shall be responsible for providing technical review of development proposals for consistency with City policies and regulations, and making advisory recommendations to the Planning Commission and City Council.

#### Planning Commission

Upon submission of the Specific Plan, a public hearing was convened on January 17, 2018 with the Planning Commission to review the Specific Plan, Zoning Code Amendment, Zoning Map amendment, and related CEQA document. After a thorough and detailed review of all aspects of the project, the Planning Commission recommended to City Council to approve in modified form, the project.

#### City Council

The City Council, as empowered by the Rosemead Municipal Code, approved the Specific Plan, Zoning Code Amendment, Zoning Map amendment, and the related CEQA document on February 13, 2018 with a second reading on February 27, 2018. The City Council found:

- The proposed specific plan is consistent with the objectives, policies, general land uses, and programs of the general plan and other adopted goals and policies of the City.

- The proposed specific plan would not be detrimental to the public interest, health, safety, convenience or welfare of the City.
- The subject property is physically suitable for the requested land use designations and the anticipated land use developments.
- The proposed specific plan shall ensure development of desirable character, which will be compatible with existing and proposed development in the surrounding neighborhood.

The adoption of the Specific Plan is also subject to environmental review in compliance with CEQA.

### Implementation Action Plan

The following Implementation Action Plan (Table 6.1) lists the specific actions that need to be taken by the City of Rosemead, in coordination with developer or public agencies, to fully implement the vision outlined in this

Specific Plan. The Implementation Action Plan summarizes each action by topical area and provides a priority timeframe, primary responsibilities, and partners. The Implementation Action Plan will be used by the City of Rosemead throughout the life of the Garvey Avenue Specific Plan, and as such should be periodically reviewed and updated by the City to reflect conditions as they change over time.

### Development Permits

All development within the Specific Plan area is subject to the Site Plan Review process as established in the City of Rosemead Municipal Code. Adoption of the Specific Plan includes adoption of the design standards and guidelines contained herein, which provide direction for the design of the development project on the project site.

### Appeals

Any discretionary action of the Community Development Director shall be final, following a ten-day appeal period, unless appealed to the Planning Commission within ten days of the Director's decision, as stated in RMC

Section 17.160. Furthermore, the Community Development Director may, in his/her discretion, refer to the Planning Commission any application for a Minor Exception or an Administrative Use Permit for the decision of the Planning Commission without further fee to the applicant. Any discretionary action of the Planning Commission shall be final following a ten-day appeal period, unless appealed to the City Council within ten days of the Commission's decision, as stated in RMC Section 17.160.

Decisions of the City Council are final.

Appeals shall be processed in accordance with RMC Section 17.160. The appeal shall be accompanied by a fee in the amount set by City Council resolution.

**TABLE 6.1: IMPLEMENTATION ACTION PLAN**

Implementation Action Plan Legend	
<b>TIMEFRAME</b>	
Short	1-5 years
Medium	5-15 years
Long	Over 15 years
Ongoing	Throughout the life of the Plan
<b>COST</b>	
\$	<\$50,000
\$\$	\$50,000-\$250,000
\$\$\$	\$250,000-\$500,000
\$\$\$\$	\$500,000-\$1,000,000
\$\$\$\$\$	>\$1,000,000

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Specific Actions		Timeframe	Enforcement Responsibility	Cost	Potential Funding Source
<b>ECONOMIC DEVELOPMENT ACTION</b>					
1	<b>Positive Business Climate Marketing.</b> In collaboration with owners of key “opportunity sites” aggressively market Garvey Avenue’s strong competitive location for a variety of retail and commercial businesses.	Ongoing	City Administration, Community Development, Property Owners	\$\$	City; property owners
<b>LAND USE ACTIONS</b>					
2	<b>General Plan Amendment.</b> Amend the City’s General Plan to achieve consistency with this Specific Plan. Changes included: Land Use Plan, Land Use Categories/Buildout Table, land use definitions, General Plan and Zoning Ordinance Consistency, Land Use and Population Estimates, Development and Population, Future Area Intersection Levels and subsequent text, Future Area Roadway Segment LOS, Level of Service Values - Intersections, Level of Service Values – Segment, and Identified Intersection Approach Improvements.	Concurrent with the Specific Plan adoption	Community Development	\$	City
3	<b>Zoning Code Amendment.</b> Amend the Rosemead Zoning Code to allow the standards and provisions in this Specific Plan to take effect. The Garvey Avenue Specific Plan’s development standards shall replace those in the Zoning Code as indicated.	Concurrent with the Specific Plan adoption	Community Development	\$	City
4	<b>Zoning Map Amendment.</b> In conjunction with the Zoning Code amendment, amend the Zoning Map to replace current zoning district designations with the new Garvey Avenue Specific Plan zones.	Concurrent with the Specific Plan adoption	Community Development	\$	City
5	<b>Garvey Avenue Corridor Master Sign Plan.</b> Consider preparing and adopting a Master Sign Plan that will improve the Garvey Avenue Corridor brand, reduce blight, and create a more uniform design throughout the corridor.	Short	Public Works, Community Development	\$	City
6	<b>Gateway Improvements Design.</b> Design new gateway entry monument signage and landscaping features to create a unified theme and clear identity for people entering the Garvey Avenue Corridor.	Medium	Public Works, Community Development	\$	City
7	<b>Gateway Improvements Installation.</b> Install new gateway entry monument signage and landscaping features in accordance with the Improvements Design (Action #6).	Medium	Public Works,	\$\$	City
8	<b>Street Furniture Design.</b> Design new street furniture that captures the “unified theme” established in the Gateway Improvements and streetscape improvements.	Medium	Public Works, Community Development	\$	City
9	<b>Street Furniture Installation.</b> Build and install new street furniture in accordance with the Street Furniture Design program (Action #8).	Medium	Public Works, Community Development	\$\$	City; Private Development
<b>FINANCING ACTIONS</b>					
10	<b>Planning and Infrastructure Cost Reimbursement.</b> Seek reimbursement for the planning costs and infrastructure planning, design, and construction incurred by the City related to the preparation and maintenance of the Garvey Avenue Specific Plan, the Garvey Avenue Specific Plan EIR, and any associated planning, open space, and/or infrastructure planning and development studies.	Short	City Administration, Community Development	\$	City
11	<b>Reimbursement District.</b> Establish a Reimbursement District that allows for a private developer to fund, construct, and/or cause to construct sewer, water, storm drainage, streets, streetscape, or public utilities infrastructure.	Short	City Administration, Community Development	\$	City
12	<b>Grant Funding Applications.</b> Consider identifying and applying for grant funding to initiate public realm mobility, streetscape, infrastructure, and gateway improvements.	Ongoing	Public Works, Community Development	\$	City
13	<b>Nexus Study for Specific Plan Development Impact Fees.</b> Undertake a nexus study to quantify the potential infrastructure needs (and related costs) of future development within the Plan area. Utilize study as a basis for updating City’s existing Development Impact Fee schedule to include additional fees unique to the Plan area (cost of nexus study can ultimately be recovered via the impact fees).	Short	Public Works, Community Development	\$	City, Private Development
<b>INFRASTRUCTURE ACTIONS</b>					
14	<b>Garvey Avenue and New Avenue Intersection Improvement Plans.</b> Prepare plans to replace right/through lane with one through lane and one right turn lane on the eastern leg.	Short	Public Works	\$	City
15	<b>Garvey Avenue and New Avenue Intersection Improvements.</b> Install intersection improvements in	Short	Public Works	\$\$\$	City; Private Development

	Specific Actions	Timeframe	Enforcement Responsibility	Cost	Potential Funding Source
	accordance with the Garvey and New Avenues' Improvement Plans (Action #14)				
16	<b>Garvey Avenue and Del Mar Avenue Intersection Improvements Plans.</b> Prepare plans to add one additional left turn lane on the northern leg; replace the right/through lanes with one through lane and one right turn lane on the eastern and western legs.	Short	Public Works	\$	City
17	<b>Garvey Avenue and Del Mar Avenue Intersection Improvements.</b> Install intersection improvements in accordance with the Garvey and Del Mar Avenues Improvement Plans (Action # 16)	Short	Public Works	\$\$\$	City; Private Development
18	<b>Garvey Avenue Roadway Reconfiguration/Streetscape Plans.</b> Prepare plans for the installation of green bulbouts and stormwater bulbouts/rain gardens, installation and/or modification of landscaped medians, modification of curb lines, installation of raised bikeways and landscape dividers, installation of shared bikeways, installation of pedestrian-oriented lighting, installation of trees and other landscaping, and restriping Garvey Avenue in accordance with the Garvey Avenue Streetscape Plan (Figure 5.1).	Short	Public Works	\$\$	City
19	<b>Garvey Avenue Roadway Reconfiguration/ Streetscape Installation.</b> Install green bulbouts and stormwater bulbouts/rain gardens, install and/or modify landscaped medians, modify curb lines, install raised bikeways and landscape dividers, install shared bikeways, install pedestrian-oriented lighting, install trees and other landscaping, and restripe Garvey Avenue in accordance with the Garvey Avenue Roadway Reconfiguration/Streetscape Plans (Action #18)	Long	Public Works	\$\$\$	City (MTA grant); Private Development
20	<b>Bus Service and Stop Improvements Coordination and Plan.</b> Coordinate with METRO to develop a corridor-wide bus stop plan that provides for additional bus stop amenities ensuring every bus stop has a bench, shade, and adequate signage.	Medium	Public Works	\$	City, METRO, Federal Funding
21	<b>Bus Service and Stop Improvements Installation.</b> Provide additional bus stop amenities in accordance with the Bus Service and Stop Improvement Plan (Action #20).	Medium	Public Works	\$\$	METRO
22	<b>Bicycle and Pedestrian Connectivity Plan.</b> Develop bicycle and pedestrian improvements plans that connect Garvey Avenue Corridor plan area to adjacent areas' bicycle and pedestrian amenities.	Ongoing	Public Works	\$	City
23	<b>Bicycle and Pedestrian Amenities.</b> Install and implement bicycle and pedestrian improvements as determined in the Bicycle and Pedestrian Connectivity Plan (Action # 22).	Ongoing	Public Works	\$\$\$	Grants (ATP, etc.)
24	<b>Shade Trees and Open Space.</b> Continue to implement carbon sequestration goals through the provision of open space and tree planting. Emphasize planting of trees to shade sidewalks, parking lots, and public plazas in order to mitigate the heat island effect and encourage pedestrian activity.	Ongoing	Public Works	\$	City
25	<b>Providing Shade Trees and Open Space.</b> Plant trees to shade sidewalks, parking lots, and public plazas as determined in the Garvey Avenue streetscape plans, the Garvey Roadway Reconfiguration/Streetscape Plans.	Ongoing	Public Works	\$	Private Development
26	<b>Sewer Infrastructure Improvement Plans.</b> Prepare plans or cause the preparation of plans to repair, retrofit, and expand sewer infrastructure addressing baseline defects and capacity deficiencies and to provide additional capacity to accommodate anticipated new land uses.	Short, Ongoing	Public Works	\$	City
27	<b>Sewer Infrastructure Improvements.</b> Repair, retrofit, and expand sewer infrastructure as specified in the Sewer Infrastructure Improvement Plans (Action #26).	Short, Ongoing	Public Works	\$\$\$	Private Development (impact fee)
28	<b>Storm Drainage Infrastructure Improvement Plans.</b> Prepare plans or cause the preparation of plans to repair, retrofit, and expand storm drainage infrastructure addressing baseline defects and capacity deficiencies and to provide additional capacity to accommodate anticipated new land uses.	Short, Ongoing	Public Works	\$	City
29	<b>Storm Drainage Infrastructure Improvements.</b> Repair, retrofit, and expand sewer infrastructure as specified in the Sewer Infrastructure Improvement Plans (Action #28).	Short, Ongoing	Public Works, Los Angeles County Flood Control	\$\$\$	City

	Specific Actions	Timeframe	Enforcement Responsibility	Cost	Potential Funding Source
30	<b>Water Infrastructure Improvement Plans.</b> Prepare plans or cause the preparation of plans to expand water infrastructure to accommodate anticipated new land use and development.	Short, Ongoing	Public Works, Golden State Water Company	\$	City, Golden State Water Company
31	<b>Water Infrastructure Improvements.</b> Expand water infrastructure to accommodate anticipated new land use and development (Action #30).	Short, Ongoing	Public Works, Golden State Water Company	\$\$\$	City, Water Companies, Private Development
32	<b>Biofiltration Treatment Features.</b> Adopt alternative standards for stormwater bulbouts/rain gardens, parking lots, and landscaped areas enabling proven biofiltration treatment features to be integrated into public thoroughfares and private development projects as provided by the Regional Water Quality Control Board.	Short	Public Works, Community Development	\$	City
33	<b>Utilities Relocation and Expansion.</b> Relocate and expand major electrical, cable, and other communication utilities as necessary to accommodate new land uses and land use patterns.	Ongoing	Public Utilities	TBD	Private Development, City

## Subdivision Maps

Approval of subdivision maps pursuant to the State Subdivision Map Act (Government Code Sections 66410-66499.58, and as may be amended) and Title 16 of the Rosemead Municipal Code may occur with or subsequent to the adoption of the Specific Plan. The project may include parcel map(s), lot line adjustments, and/or other subdivision actions.

## Specific Plan Amendments

Amendment to the Specific Plan may be requested by the applicant or by the City at any time pursuant to Section 65453(a) of the California Government Code and Rosemead Municipal Code Title 17, Article 5, Chapter 17.150. Amendment shall be processed pursuant to the provisions of the Government Code for Specific Plan and the City of Rosemead Municipal Code.

## General

Approval of this Specific Plan by the City Council is considered acceptance of the general framework and specific development standards contained within Garvey Avenue Specific Plan. The following section describes a process for

changes, in the event a Specific Plan amendment request is made.

## Substantive Changes - Defined

As determined by the Community Development Director, any substantive changes will require a Specific Plan Amendment. Substantive changes include:

- Any increase in the residential density or nonresidential intensity.
- Any proposed expansions of the geographic area included in the Specific Plan.
- Any departures from the design standards and/or guidelines that significantly change the overall character or appearance of the project.

A proposed Specific Plan amendment shall reflect the comprehensive analysis that has been undertaken in Specific Plan's adoption and may require additional environmental review. As a condition of consideration for any Specific Plan amendment, it shall be the applicant's responsibility to:

- Demonstrate the proposed amendments will meet the goals and objectives of Garvey Avenue Specific Plan and the General Plan;
- Update any technical studies and/or provide additional environmental studies as determined by the Community Development Director and incorporate all mitigation measures into the project design; and
- Provide revised Specific Plan text and maps (where relevant) that reflects the amendment requested.

## Authority to Approve Minor Modifications

The Community Development Director may approve minor modifications to the Specific Plan without a Specific Plan Amendment. Minor modifications shall include:

- Changes to landscaping/hardscaping/streetscape.
- Changes in building materials and related architectural treatments.

- Any other modification determined by the Community Development Director deemed to be minor in that it would not materially change the project approved by the City Council.

### **Maintenance and Monitoring**

As the conditions present in 2018 are anticipated to change throughout the implementation of this Specific Plan, periodic maintenance and monitoring of the Plan will be necessary. The Specific Plan shall be reviewed at least every five years to assess progress towards implementation, and if necessary, identify new or revised implementation measures to meet Specific Plan objectives. The review shall examine development projects completed and/or proposed, capital improvement projects completed and planned, and any other programs or projects that are or have been implemented. The review shall be presented to all appropriate City advisory bodies.

